

The following is presented as a draft document for the purposes of soliciting public feedback on the draft prior to this being formally considered by Council later in 2024.

It is intended as a rough, initial draft. Based on the feedback of this document through the public engagement process, a more refined draft will be prepared for formal consideration and further public review and comments.

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Introduction

Content will be provided here in a later draft of the document.

Context & Background

This provides background information on the current situation and trends in the community relevant to each of the draft sections. Content will be provided here in a later draft of the document.

First Nations

This is a new section that addresses the City's and the community's connections and relationships to regional First Nations governments. City staff have been working directly with the Adams Lake Indian Band and the Neskonlith Indian Band on the initial content of this section. This work is underway and ongoing, and will be presented in a later draft of the document.

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Growth Management

Growth Management Objectives

To create a range of housing opportunities and choices.

To emphasize the need for a mix of land uses.

To maintain a balance between residential and non-residential land uses.

To maintain the housing supply.

To preserve open space, agricultural land and critical environmental areas.

To strengthen and direct development towards existing developed areas and communities.

To create walkable and accessible communities.

Growth Management Policies

1. Continue to use the Urban Containment Boundary (UCB) (see Map 4.1 Urban Containment Boundary) as a primary planning tool, supporting new growth within the UCB and discouraging urban development and subdivision outside the UCB.
2. Monitor and ensure that the Official Community Plan designates adequate land areas within the UCB to support long-term growth.
3. Review the Zoning Bylaw and Subdivision and Development Bylaw to consider the stratification of Small Scale Multi Unit Housing.
4. Discourage additional single family subdivision in Areas A and B. Rezoning to multi family residential zones is strongly encouraged.
5. Support ALR Exclusion applications for the Industrial Park and the "ALR Island" as identified on Map 4.2. ALR exclusion in these areas will be subject to pre-planned road network and infrastructure plans and rezoning for intended uses.
6. ALR exclusion applications must be made by the City and supported only in those areas identified in this plan. As part of the exclusion application the "ALR Island" is to be redesignated to High Density Residential. Subsequent down zoning of the "ALR Island" would not be supported.
7. As part of the ALC Exclusion application for the "ALR Island," or shortly after, a neighbourhood plan for the area is required.
8. ALR exclusion applications outside of the Industrial Park and the "ALR Island" are not supported unless significant community benefits are proposed.
9. Encourage all growth to be sensitively integrated with neighbouring land uses and resources, particularly natural areas, and agricultural and residential lands, using Development Permits as one of the tools.

10. Strive to protect lands within the ALR for agricultural use, particularly the Salmon River Valley Agriculture area with its high quality agricultural soils, large land parcels and established agricultural base.

11. Protect the forested hillside areas of Salmon Arm as a valuable renewable resource and as a vital part of the City's watershed, recognizing that in their forested state these areas contribute significantly to Salmon Arm's natural landscape context.

12. Protect from development Salmon Arm Bay, the Shuswap Lake foreshore, natural landscape features (including but not limited to Little Mountain, Mount Ida, Larch Hills, Fly Hills and Turner Creek), and numerous watercourses as they form part of the unique natural beauty of Salmon Arm.

13. Continue to support a wide range of housing types and tenures in order to meet the needs of the diverse lifestyles and aging population, including affordable housing, sensitively integrated infill and intensification of existing development areas.

14. Continue to recognize and support the Downtown as the principal commercial, business, cultural and administrative centre, encouraging it to be a vibrant, compact, accessible downtown that reinforces the comfortable, safe atmosphere of Salmon Arm, including opportunities for upper floor dwelling units as part of mixed use developments.

15. Continue to support other commercial areas in the community which add to the variety of retail and service opportunities, east and west of the Downtown along the Trans Canada Highway between 30 Street SW and 30 Street NE.

16. Continue to provide a high standard of municipal services, including an efficient public transportation system that supports compact community development.

17. Continue to encourage local amenities such as schools, care centres, shelters, parks and local commercial uses in residential neighbourhoods, recognizing that changing employment structures (e.g. small independent home-based businesses) may increase demand for local neighbourhood services.

18. Continue to improve community services and quality of life through the following actions:

- planning for recreation, parks and greenway land and facilities;
- encouraging community involvement in planning through interesting and accessible consultation processes; and
- support continued cooperation in the provision of arts, culture, social, health care, and other community facilities, e.g. community care, assisted living facilities .

Urban Containment Boundary Objectives

To complete the servicing of lands within the residential, commercial and industrial lands within the UCB.

To prioritize development and community growth within those areas that have existing infrastructure.

To support a compact community where people can connect easily to the areas that they work, live and play.

Growth Priorities Within the UCB (Map 4.1)

1. Area A – High Density Residential & Mixed Use High Density

The City supports and strongly encourages residential and mixed use development in this area. Water, sewer, storm and road networks are most complete within this area. In order to support further growth in this area the City will prioritize connecting and improving infrastructure in this area before extending services in other growth areas.

2. Area B – High and Medium Density Residential

The City supports infill development within this areas and recognizes the need to support growth by connecting and improving priority community infrastructure facilities.

3. Area C – Low density (Raven and parts of Foothills between 10 Avenue SE and 14 Avenue SE)

The City recognizes the limits of infrastructure to support growth in this area. The City will support limited infill in these areas recognizing that existing water, storm, sewer and road networks limit growth. Infrastructure growth to facilitate development will be via development and infrastructure expansion by the City will not be prioritized in this area.

4. Area E – Employment Lands (commercial & industrial)

The City recognizes the need for complete services in this area to support future growth potential of economic lands. The expansion of infrastructure into this area will be supported by cost sharing between the City and development.

5. Area Outside of Areas A, B, C & E

The City recognizes that area within the UCB but not prioritized as above may experience growth; however, servicing in this area is incomplete. Infrastructure growth in this area will be completed entirely by development. The City may not support development in this area until in-fill development in Areas A, B and C is complete.

Urban Containment Boundary Policies

1. The UCB contains low, medium, high and mixed use residential densities and employment lands. Growth within the UCB is supported with development prioritized in those areas designated for Medium to High density then in areas of low density. In the expansion of infrastructure and services, employment lands are prioritized.

2. Residential densities are not supported outside of the UCB.

3. Review the Zoning Bylaw to modernize commercial uses and floor area ratios to encourage mixed use development.

4. Support mixed use and high density residential development in the highway corridor, town centre areas by zoning lands to accommodate multi-storey buildings and uses appropriate to the designation.

5. Limit the support of servicing variances to development within Area A for infill development. Servicing variances within Area B may be supported when community amenities such as lighting, fire hydrants, road dedication, trails and/or multiuse paths are provided by the developer and fully constructed. Servicing variances not supported for construction of connecting trails and/or multiuse paths.

6. Prioritize capital projects and road network planning within Area A, B and E.

Municipal cost sharing programs, including expenditure of Development Cost Charge funds and capital works programs, may be directed towards the current priority Area A (and as Council deems appropriate).

7. When sufficient infilling has occurred within Area A, B and E, municipal cost sharing programs, including Development Cost Charge expenditures and capital works programs, may be allocated to Area C, as determined by Council and City Bylaws. Sufficient infilling means that development of the developable land area is at least 80% complete in the context of the overall area being addressed, based on the City's determination.

8. Prior to consideration of the allocation of municipal cost sharing programs to Area C, including Development Cost Charge expenditures and capital works programs, review the high level municipal water, sanitary, and stormwater servicing requirements for the area, including downstream capacities.

9. The following are additional policies specific to Development Areas B and C in the Foothills Road SW area:

- a. No development above the transmission power line;
- b. Large minimum lot sizes and clustering development on the uphill side of the future east-west urban collector road between 10 Street/Foothill Road SW and 10 Street/20 Avenue SE; and,
- c. Cluster development on mid elevations.

Urban Residential

Urban Residential Objectives

To provide a range of housing for all that meets the needs of the community, supports sustainability, balanced with green and natural areas, while maintaining a small-town community feel and supporting community growth.

To provide an adequate supply of housing.

To provide opportunities for a variety of housing types, tenures and densities in appropriate locations close to services and amenities.

To facilitate the development of purpose built rental housing that provides security of tenure.

To encourage and support affordable and special needs housing, including housing options for the community's diverse population.

To encourage residential development that will support strong neighbourhoods in compact communities.

Urban Residential Policies

General Policies

1. Encourage residential development within the Urban Containment Boundary (UCB) that creates a mix of residential housing types, tenures and densities.
2. Urban residential development shall be in alignment with the Community Housing Strategy and Housing Needs Assessment, as updated from time to time.
3. Urban residential densities are not permitted outside the UCB.
4. All multi-family housing developments creating more than three units are required to obtain a Residential Development Permit (Section 16 Development Permit Areas).
5. Prioritize development applications for new purpose built rental housing and affordable housing.
6. Continue to provide municipal fee reductions for development applications for new purpose built rental housing and affordable housing.
7. Consider Development Cost Charge reductions and waivers on a case by case basis or through the review of the Development Cost Charge Bylaw.
8. Support and facilitate the development of new purpose built rental housing.
9. Support non-profit and housing organizations in the development of new affordable housing units.
10. Work with non-profit and housing organizations and BC Housing and other funding agencies to facilitate the development of new affordable housing units.

11. Support and facilitate the adaptive re-use and re-development of faith-based religious institutional properties and buildings for affordable or cooperative housing (including the use of density bonuses) should they cease religious services.
12. Maintain residential areas free from intrusion of incompatible uses, e.g., industrial uses.
13. Incorporate environmental considerations, transportation plans and parks, open space and greenways as integral components in planning for urban residential uses.
14. Review and update the Zoning Bylaw to define the appropriate special amenities and related density bonus provisions based on local needs and economic conditions.
15. The application of new R-11 zoning to additional properties following the adoption of this Bylaw will be strongly discouraged.
16. Develop a secondary housing market monitoring report to monitor all rental housing vacancy and rental rates.
17. Review Zoning Bylaw mixed use, residential high and medium density building heights.

High Density Residential Policies

1. High Density Residential areas are designated on Map A-1 Land Use.
2. High Density Residential areas may accommodate all forms of residential multiple family dwelling housing up to and including apartment buildings.
3. High Density Residential A developments may be permitted to a density of 130 units per hectare. Density may be increased to a maximum of 200 units per hectare for affordable or purpose built rental housing (at least 50% of units), or 150 units per hectare for multiple family housing, multiple family housing in mixed use buildings, or Assisted Living Housing, subject to the provision of special social or public amenities (e.g. commercial childcare facility, fully accessible dwelling units and suites, rental housing, affordable rental housing, below grade or parkade style parking, parkland, greenways or trails, green building and site design) in accordance with the Zoning Bylaw.
4. Mixed Use Development in the City Centre / Downtown and Highway Service/Tourist Commercial designated areas may include residential units permitted to the High Density Residential density provisions as specified above and in accordance with the Zoning Bylaw.

Medium Density (High Density B) Residential Policies

1. Medium Density Residential areas are designated on Map A-1 Land Use.
2. Medium Density Residential areas may accommodate all forms of multiple family residential housing including ground oriented dwellings, rowhouses, apartment buildings and small scale multiunit housing (including single family dwellings and duplexes, secondary suites and accessory dwelling units).
3. Medium Density Residential areas may be permitted to a density of 88 units per hectare. Density may be increased to a maximum of 110 units per hectare for affordable or purpose built rental housing (at least 50% of units), or 100 units per hectare for multiple family housing, multiple family housing in mixed use buildings, or Assisted Living Housing subject to the

provision of special social and/or public amenities (e.g. commercial childcare facility, fully accessible dwelling units and suites, below grade or parkade style parking, additional parkland, greenways or trails, green building and site design) in accordance with the Zoning Bylaw.

4. Mixed Use Development in Neighbourhood Commercial designated areas may include residential units permitted to the Medium Density Residential density provisions as specified above and in accordance with the Zoning Bylaw.

5. Discourage future subdivision for large lot developments (Single Family Dwelling Units) within the Urban Containment Boundary, especially if they would preclude higher density subdivisions in the future.

Non-Residential Uses Policies

1. Encourage limited amounts of neighbourhood commercial use providing local services in all Residential designations, through a review of the Zoning Bylaw.

2. Assisted Living Commercial uses may be permitted as an accessory use in multi-family developments in High and Medium Density Residential designations.

3. Home Occupations are permitted in all Residential designations.

Siting Policies

1. Encourage Residential developments and subdivisions to meet the following criteria:

- a. good access to transportation routes, including transit, trails and active transportation, and roads;
- b. good access to recreation, parks and open space;
- c. good access to community services, e.g. commercial uses and school sites;
- d. sufficiently removed from incompatible land uses to ensure the health, safety and welfare of the residents; and
- e. capable of being serviced with municipal, private and Crown utilities including fire protection, in accordance with City standards and specifications.

2. Encourage siting of residential developments and subdivisions to:

- a. take advantage of and preserve special site features such as natural vegetation, mature landscaping, and topographic features;
- b. provide for safe, convenient and separated active transportation and vehicular routes on collector or arterial corridors and/or greenways; and
- c. provide opportunities for interaction with natural surroundings and mature vegetation retention including the provision of trail connections.

3. Minimize the conflicts between agricultural and residential land uses through the use of access restrictions, buffers, fencing, and trail corridors for residential developments adjoining agricultural properties.

Housing Diversity Policies

1. Follow the 2020 Community Housing Strategy, with community partners, identifying opportunities to encourage and support rental housing and non-market housing needs, as well as homelessness initiatives.

2. Support a coordinated approach to addressing housing issues and liaise with federal and provincial governments, non-profit organizations and community groups in this regard.

3. At the rezoning stage / in the rezoning process, encourage developers to make a percentage of all new housing units or lots available for rental, affordable and special needs housing, with potential for a corresponding density bonus.

4. Support community support services and uses, such as shelters, transition / youth homes and other forms of social housing within the Urban Containment Boundary in locations near local services.

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Rural and Agriculture

Rural and Agriculture Objectives

To maintain the rural and agricultural character and land use pattern of open space, agriculture, forestry and rural/country residential lands outside the Urban Containment Boundary.

To protect farm land and farm operations.

To minimize the impact of urban development on agricultural land.

To support agriculture on both ALR and non-ALR land.

To preserve land with agriculture potential in the ALR.

To support agri-business opportunities in rural and agricultural areas that are complementary to local agriculture, including agri-tourism and agri-food businesses.

To improve local food security.

To encourage and support the expansion of local food production and processing.

To incorporate urban agriculture within the community.

To carefully manage rural resources, including resource extraction, in partnership with provincial agencies.

To plan for the preservation of open spaces, natural areas, and scenic views in rural areas.

Rural and Agriculture Policies

General Policies

1. Acreage Reserve, Salmon Valley Agriculture and Forest Reserve areas are designated on Map A-1 Land Use.
2. Discourage additional development, particularly at urban densities, in the Acreage Reserve, Salmon Valley Agriculture and Forest Reserve designations. Rural residential density is 2 units/parcel or as limited by the Agricultural Land Commission.
3. Maintain or enhance the configuration and size of parcels designated Acreage Reserve, Salmon Valley Agriculture and Forest Reserve through boundary (lot line) adjustments and/or consolidations; rezoning, subdivision and/or Agricultural Land Reserve exclusion applications are not supported.
4. Support boundary (lot line) adjustments which bring lot sizes more in compliance with the regulations of the City's Zoning Bylaw throughout the Acreage Reserve, Salmon Valley Agriculture and Forest Reserve designations. Boundary adjustments should not add to the degree of non-conformity of any lot.
5. Support road widening and dedication for the safe movement of vehicles, when road dedication is supported by the ALC.

6. Strata titling or subdivision of Accessory Dwelling Units in the Agricultural areas is not supported.
7. Municipal utilities in the Forest Reserve, Salmon Valley Agriculture and Acreage Reserve designations should not exceed the existing standard or be extended, except for the municipal water system outlined in Section 14 Utilities and Infrastructure and when supported by the ALC.
8. Support community efforts to create a water system or irrigation district for the supply of water to the Salmon River Valley
9. Support community efforts to create a producer / agricultural representative organization or an agricultural advocacy organization (such as a “farmer’s institute”).
10. Support opportunities to enhance community safety measures – road safety, managing road and trail users, crime prevention in rural or remote areas.
11. Work collaboratively with producers and partners to plan for land areas for the purposes food processing within agricultural areas or industrial areas. Where suitable land cannot be located within the City boundary, the City will work collaboratively with producers and the CSRD.
12. Encourage and education community backyard food gardens and agriculture, including small animal and plan disease management
13. Support emergency management programs for the sheltering of animals/livestock on ag lands/Fall Fair Grounds.
14. Undertake a review of the Zoning Bylaw and the Pound and Animal Control Bylaw to determine appropriate regulations to allow for urban agriculture and the keeping of animals and livestock within the Urban Containment Boundary.
15. Support leasing of lands for agricultural production within the City and regionally.

Agricultural Land Reserve Policies

1. The City supports and recognizes the importance of agricultural land and food production on lands currently in production and land production in the future. These lands are a source for local food and employment in the community. In considering development applications on ALR lands the City will include in its consideration:

- the size of the current farming operation (including leased lands);
- types of commodity(ies) produced soil classification;
- number of farm workers employed by the operation;
- land remediation plans for any areas identified for temporary uses; and
- Area limits for accessory buildings for farm use consistent with the polices of the ALC and BC and National Building Code.

2. The City supports minimizing the impact of residential development on ALR land and, consistent with ALC polices, encourages the following principles for residential development in the ALR:

- Additional, and temporary, farm worker housing approved by the ALC is supported;
- Principle dwelling units greater than 500m² are not supported;

- Accessory Rural Dwellings greater than 90m² are not supported;
- Clustering of residential development adhering to residential footprint(s) of the ALC;
- Encourage driveways for residential development to be less than 60m from highway or road frontage; and
- Area limits for residential accessory buildings in the Zoning Bylaw.

3. ALC applications to subdivide land in the ALR under Section 514 (Subdivision to Provide Residence for a Relative) of the *Local Government Act*, should not be supported for parcels less than 8.0 ha.

4. Applications to subdivide land outside the ALR, under Section 514 of the *Local Government Act* (Subdivision to Provide Residence for a Relative), may be supported on parcels greater than 8.0 ha, as outlined in the City's Zoning Bylaw.

5. ALC Exclusions are only supported in the areas identified in Map 4.2. These areas have been identified for the developments of the Industrial Park and High Density residential and commercial development.

6. Prior to the City's application for an ALC exclusion, undertake a review of the future potential land uses and servicing requirements of ALR lands surrounded by 10 Avenue SW, Shuswap Street, Foothill Road SW, and 10 Street SW in conjunction with landowners, the ALC, and the community at large. As part of the review, ensure that lands with high agriculture capability continue to support agriculture uses.

7. Home-based businesses are supported in the Acreage Reserve, Salmon Valley Agriculture and Forest Reserve designations subject to relevant zoning, licensing and ALC Regulations.

8. The City will work collaboratively with the ALC to mitigate the impact of Active Transportation routes through agricultural and rural areas, agricultural operations, and transportation routes through sensitive routing, signage, fencing and buffering measures.

Agriculture Policies

1. Agriculture, including but not limited to agricultural food production, forage crops, livestock operations and accessory commercial uses, is permitted in the Acreage Reserve, Salmon Valley Agriculture and Forest Reserve area designations.

2. Recognize the importance of agriculture to the economy and support industry practices as identified in the Farm Practices Protection (Right to Farm) Act.

3. Support the maintenance and enhancement of lands for agricultural use within the Agricultural Land Reserve.

4. Either a conventional secondary suite contained within a dwelling or a detached suite is supported as an accessory residential use in the Acreage Reserve, Salmon Valley Agriculture and Forest Reserve designations.

5. Encourage and support the development and expansion of alternative, smaller scale agricultural businesses, such as production and marketing of locally grown foods, organic farms, on-site sales of on-site and locally grown food, incentives for farmers to remain on their land, and support for new farmers to obtain land.

6. Research opportunities to permit enhanced agri-tourism consistent with Ministry of Agriculture and ALC policies and programs.
7. Research opportunities to promote urban agriculture, such as community gardens and polices that allow small scale non-intensive farming within the Urban Containment Boundary.
8. Encourage contiguous space open space within residential development intended for garden space for residents.
9. Encourage education programs dealing with agricultural practices that enhance natural ecosystems and protect environmental resources, e.g., Environmental Farm Planning, and support the agriculture industry in applications for grants for study or implementation of environmentally sound agriculture practices.
10. Work with CSRD to address non-native and noxious weed control in agricultural areas, including within boulevards and drainage corridors.
11. Work with farmers and federal and provincial agencies to establish strategies to limit livestock access to domestic and recreational source waters.
12. Consider creation of an Agriculture Plan to provide strategies and policies to preserve and enhance agricultural lands outside of the Urban Containment Boundary.
13. Consider the creation of an Agricultural Plan that focuses on agricultural resources identifying challenges and practical solutions to those challenges.
14. Consider creation of Food Systems Plan to support and promote food security.

Mineral and Aggregate Resources Policies

1. Mineral and aggregate processing may be permitted in areas of high aggregate potential, as identified on Map Aggregate Potential or based on site investigations, subject to zoning or the issuance of a Temporary Industrial Use Permit.
2. Encourage the Ministry of Energy and Mines to engage the City and public in meaningful dialogue when reviewing, issuing, or amending mining licenses.
3. Encourage the Ministry of Energy and Mines to minimize conflicts among mineral licence activities and adjacent lands, watersheds and riparian areas.

Acreage Reserve Policies

1. Country Inn Commercial Uses may be supported within the Acreage Reserve designation subject to both **Section 6** Rural and Agriculture and the policies or approval of the ALC for lands within the ALR.
2. Subdivision to accommodate further rural small agricultural holdings may be supported within the Acreage Reserve area situated west of both the Salmon River and the Trans Canada Highway subject to compliance with the following criteria:
 - the site is well drained and free from flooding, unstable soils or other hazardous conditions;
 - soil conditions permit permanent on-site sewage disposal for each parcel as determined

- by the appropriate agency;
- availability of adequate potable water supply on each parcel, approved by the appropriate agency;
 - minimum two (2) hectare parcel size and subject to appropriate zoning; and;
 - the lands have an improved agricultural soils capability rating of class four (4), class five (5), class six (6) or class seven (7) as determined by the ALC, based on the Canada Land Inventory Agricultural Capability Classification System. The principle of protecting better quality agricultural soils from subdivision into small lots will guide decisions on mixed category applications and the ALC will continue to consider each subdivision application on its own merits to ascertain the potential impact of subdivision on existing and potential agricultural development.

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Commercial

Commercial Objectives

To continue to promote the Downtown as the commercial and cultural focus of the community.

To support a walkable downtown through safe and efficient pedestrian infrastructure.

To limit highway commercial development to lands within the Urban Containment Boundary and in close proximity to the Trans Canada Highway.

To preserve a reasonable supply of commercial land.

To continue to support the development of home-based businesses.

To support the increased development of small scale local commercial businesses in residential neighbourhoods.

Commercial Policies

General Commercial Policies

1. Continue to support the development, redevelopment, and infilling of Downtown and Highway Service/Tourist Commercial designated lands along the Trans Canada Highway between 30 Street SW and 30 Street NE as the primary commercial areas in the City.
2. Minor expansions to boundaries of the Downtown and Highway Service/Tourist Commercial designations may be considered. Expansions to the Downtown may be considered adjacent to and in proximity to the Downtown area. Expansions to the Highway Service/Tourist Commercial designations may be considered adjacent to and in proximity to existing Highway Service/Tourist Commercial designations within the Urban Containment Boundary.
3. The Downtown, Highway Service/Tourist Commercial, and Neighbourhood Commercial areas are designated as Development Permit Areas and are subject to the guidelines presented in Sections 16 Development Permit Areas.
4. Support tourism and recreation through the expansion of commercial options near the waterfront and consideration of wharf access and uses into the future.
5. Create affordable housing options in commercial areas in order to attract and retain employees.

Downtown Commercial Policies

1. The Downtown area is designated on Map A-1 Land Use.
2. Support development in the Downtown area that includes pedestrian-oriented retail, food service, government, entertainment, cultural, recreational, office and business uses.
3. Support residential uses in the Downtown when located above the street level. Residential uses may also occur at street level when they are associated with Live/Work developments, on building frontages where commercial uses may not be viable, and where the commercial use component takes precedence and contributes to the overall commercial land use inventory.

4. As part of an upcoming review of the Zoning Bylaw:

- Encourage additional residential density Downtown to support commercial uses.
- Encourage Live/Work options.
- Consider expanded recreational opportunities.
- Consider reduced or eliminated parking requirements.
- Consider floor area requirements to ensure substantial ground floor commercial development in mixed use buildings.

5. Encourage developments in the Downtown to achieve a high development density, using methods such as 100% parcel coverage, zero lot line setbacks, underground/under-building parking, and upper floor dwelling units.

6. Continue implementation of the Revitalization Tax Exemption Bylaw within the "Revitalization Tax Exemption Area" in the Downtown area.

7. Continue to support and regulate outdoor seating and patios for restaurants and cafes in the Downtown.

8. Work with the Salmon Arm Economic Development Society and Downtown Salmon Arm to encourage extended hours for Downtown businesses into the evening.

Highway Service/Tourist Commercial Policies

1. The Highway Service/Tourist Commercial area is designated on Map A-1 Land Use.

2. Encourage land uses in the Highway Service/Tourist Commercial area which are distinguished by an orientation toward access by vehicular traffic. Uses included are automotive services, tourist accommodation, entertainment and recreational tourist services, minor repair, retail commercial warehousing, retail and food outlets, and upper floor dwelling units.

3. The Highway Service/Tourist Commercial area is a gateway to the community and it is important that each development contributes to a positive image of the community.

4. The Highway Service/Tourist Commercial area is divided into six unique areas that recognize historical land use patterns and ensure long-term land use compatibility between unique development areas. The strategic directions for these locations are:

Urban Commercial

1. Commercial Corridor West of City Centre to 30 Street SW – The Highway Service/Tourist Commercial area on the west side of the Downtown is oriented toward vehicle service and retail warehousing uses. Activities have relatively low site coverage (e.g. automotive and recreation vehicle sales, large buildings and associated parking lots). This area serves the needs of the community and region. Limited expansion of this area may be considered.

2. 10 Street SW and 10 Avenue SW Commercial Area – This area is primarily a shopping centre with retail food and general retail uses. Additional commercial and mixed use developments are appropriate in this area. This area also serves the commercial needs of the surrounding residential areas including future residential development in Residential Development Area B. Limited expansion of this area may be considered.

3. Commercial Corridor East of City Centre to 30 Street NE– The Highway Service/Tourist Commercial area east of the City Centre is developed with tourist accommodation, retail food, general retail, office, medical services, entertainment and community recreation, mixed use, and education facilities. This area also serves the commercial needs of the surrounding residential areas and greater community. Limited expansion of this area may be considered.

4. Canoe Highway Commercial Area – The commercial area near Canoe along the Trans Canada Highway is oriented toward highway tourist uses. Appropriate uses include motels, RV Parks, campgrounds, recreation, service commercial uses, and mixed use development. Limited expansion of this area may be considered.

Rural Commercial

1. Glen Echo Commercial Area – This area is recognized as a historical Highway Service/Tourist Commercial area supporting tourist and recreational resort uses. This area is not on the municipal sewer system, therefore uses generating high volumes of waste water are discouraged. Expansion of this commercial area beyond its current boundaries is not supported.

2. Trans Canada Highway/Highway 97B Intersection – This area is recognized as an existing Highway Service/Tourist Commercial area. New developments will need to address access issues and apply a high development standard appropriate to this gateway location. This area is not on the municipal sewer system, therefore uses generating high volumes of wastewater are discouraged. Service and tourist commercial uses that cater to the travelling public on the Trans Canada Highway and Highway 97B may be supported. Expansion of this commercial area beyond its current boundaries is not supported as surrounding lands are in the ALR.

3. Encourage active transportation and transit connectivity to, from, and between Highway Service/Tourist Commercial areas.

Neighbourhood Commercial Policies

1. New Neighbourhood Commercial areas may be located within High, Medium and Low Density Residential areas provided that the development is intended to service the local convenience shopping needs of the surrounding residential area, and permitted uses may include but are not limited to:

- convenience or small scale retail store,
- neighbourhood pub, restaurant or café,
- commercial child care,
- office,
- personal service establishment.

2. Support innovative redevelopment of residential areas for Neighbourhood Commercial developments in close proximity to the 50 Street NE and Canoe Beach Drive NE intersection in Canoe, and the Downtown.

Additional Commercial Uses Policies

1. Country Inn Commercial uses may be located within the Acreage Reserve designation subject to zoning and ALC regulations, where required, as well as the use addressing unique site, tourist, agri-tourism, environmental or outdoor recreational opportunities.

2. Assisted Living Commercial uses may be permitted as accessory uses in multi-family developments on lands designated for High Density and Medium Density Residential Use.
3. Home Occupation uses may be permitted in all urban residential and rural/agricultural areas, as specified in the Zoning Bylaw. A maximum of two Home Occupations may be permitted per lot.
4. Bed and Breakfast operations may be permitted in all urban residential and rural/agricultural areas as specified in the Zoning Bylaw and ALC policies if in the ALR.
5. Draft regulations or policy regarding short term rentals (or the commercial use of residential property).

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Industrial

Industrial Objectives

To promote industrial businesses that are complementary to existing industrial areas.

To support industrial land uses that are similar to each other in specific locations.

To protect existing industrial lands and intensify uses for job creation.

To support skills training and educational opportunities within the industrial areas.

To limit residential development in industrial areas.

To support long term planning for transportation access points with highways and arterial roads and road network planning for future growth.

To support transit connections to industrial areas.

Industrial Policies

General Policies

1. Three industrial land use categories are designated for industrial development as shown on, and as follows:

- a. Light Industrial;
- b. General Industrial; and
- c. Airport Industrial/Commercial.
- d. Industrial Special Development Area)

2. Permitted uses for Industrial designated areas include:

- a. Light Industrial – oriented toward smaller, site specific, enclosed manufacturing operations that require minimal outside storage of materials or product, that have few impacts on adjacent properties, where sites are made compatible with adjacent non-industrial uses through screening, landscaping and buffering measures. Remove Residential uses
- b. General Industrial – oriented toward more extensive manufacturing activities that require large areas of on-site storage of resource materials and finished product and ease of access to major arterial highways. General industry may include the storage and processing of raw materials, such as logs and wood products, sand/gravel, concrete and minerals, metallic industries and petroleum products. Light industrial uses can be considered in the general industrial designation. General industrial activities require special attention by reason of their potential impact on the adjacent properties and natural environment. Remove residential uses
- c. Airport Industrial/Commercial – oriented toward industrial and commercial uses requiring direct, convenient access to airport facilities. All commercial air operations, except helipads and floatplane operations as referenced in policies in Section 13 Transportation, should be located in this area.
- d. Industrial Special Development Area – those lands north of airport that will continue to allow for the existing residential development to continue until rezoned.

3. Update and amend the industrial zones in the Zoning Bylaw to more accurately reflect the appropriate uses of the Light Industrial, General Industrial and Airport Industrial/Commercial designations outlined above, and to minimize the potential for residential development.

4. Review the Zoning Bylaw regulations for storage facilities and locations for new storage locations (indoor and outdoor). Limit the development of warehouse and mini warehouse development to Light Industrial designated areas.
5. Support small scale/neighbourhood commercial in industrial areas that will provide services to employees of industrial uses.
6. Research options and opportunities for servicing one or more industrial areas of the City with municipal services, including storm drainage and sanitary sewer services.
7. Create an industrial development Plan for the creation of area specific servicing, with implementation funding via Local Service Area or other funding strategy.
8. Review the Subdivision and Development Servicing Bylaw to confirm the appropriate servicing standards for the Light Industrial, General Industrial, Airport Industrial/Commercial and Industrial Special Development Area designations.
9. Minimize, through appropriate zoning, the intrusion of primarily commercial, retail and retail service uses in the industrially designated areas and encourage infilling of purely industrial and related ancillary uses to preserve a reasonable supply of industrial land.
10. Where industrial uses require ancillary retail commercial, limit the latter to 25% of the maximum gross floor area, contained within the principal building.
11. Parcels rezoning to industrial uses within the designated Industrial Areas should meet the following criteria:
 - a. Access to arterial or collector roads;
 - b. Capable of being serviced with municipal, private and Crown utilities, in accordance with City standards and specifications. On-site servicing may be considered if extension of services is not possible; and
 - c. Capable of being sufficiently buffered from adjacent non-industrial land uses to reduce potential conflicts.
12. To ensure appropriate form and character of industrial development fronting major transportation corridors, all Light Industrial, General Industrial and Airport Industrial/Commercial designated lands fronting the Trans Canada Highway, Highway 97B, 10 Avenue SE or the north side of 20 Avenue SE are designated as an Industrial Development Permit Area per Section 16 Development Permit Areas.
13. Support increased public transit (or rideshare services) to and from the main industrial park, with schedules that align with business shifts. Explore park and ride, rideshare muster points outside of industrial park and locations for end of trip facilities within industrial park to support shared riding or active transportation.
14. Encourage active transportation routes connecting residential areas with Industrial areas, and support the development of community end of trip facilities in industrial areas.
15. Support phased industrial development on rural residential properties designated for Light Industrial use in the Industrial Special Development Area and for General Industrial use south of the airport along Auto Road, subject to appropriate zoning. Subdivision will only be considered where it supports the creation of new parcels zoned for industrial uses.

15. Support cannabis production and processing in industrial park through an amendment to the Zoning Bylaw, and remove the use from commercial areas.
16. Continue to support the use of a Revitalization Tax Incentive program for industrial areas.

Industrial Special Development Area Policies

1. Industrial lands within the Special Development Area are shown on Figure 1.

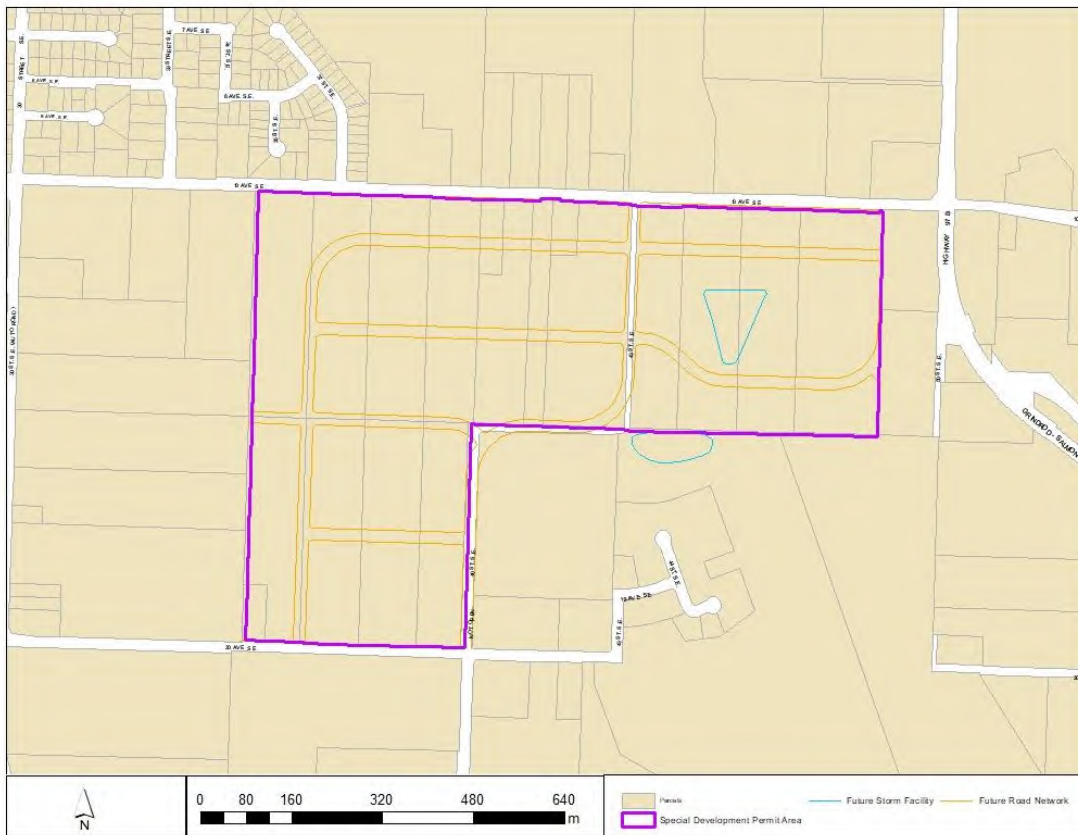


Figure 1: Industrial Special Development Area

2. The lot layout and location of infrastructure facilities should generally be aligned with the layout indicated in Figure 1.
3. Support the exclusion of the lands in the Industrial Special Development Area from the ALR. The ALC has given preliminary approval for exclusion of these lands (Resolution #109/88) A block-style Exclusion application by the City and rezoning of lands to Industrial Holding Zone
4. Prepare light industrial zoning, servicing standards, vehicle traffic and access requirements, and an expanded form and character Development Permit Area to address a higher standard of light industrial development in the Special Development Area, taking into consideration the adjacent residential, park and airport uses.
5. Consider a Local Service Area for the development of infrastructure and servicing in the Industrial Special Development Area.

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Environmental

Environmental Objectives

To promote the enhancement and preservation of natural areas, habitats, and networks of green spaces, fostering a robust level of biodiversity.

To promote landscaping that incorporates climate-resilient plant species native to the region, promoting biodiversity and resilience to local environmental conditions.

To conduct City activities and operations to minimize impacts on the environment and strive to continuously improve the City's environmental performance, setting a precedent through leadership.

To direct all new development to protect and respect environmentally sensitive areas.

To encourage new developments to prioritize energy efficiency and prioritize the adoption of renewable energy sources.

To raise awareness about the environment and encourage actions that safeguard and improve ecological systems, biodiversity, and watershed protection, setting a precedent through leadership.

To protect the ecological integrity of watercourses.

To manage and prevent the proliferation of invasive species.

To reduce greenhouse gas emissions.

To support public transit services and active transportation.

To encourage local food production and processing.

To restrict or limit development and access to sensitive watersheds that are sources of municipal drinking water.

To improve air quality by increasing tree canopy coverage and promoting the ecological benefits of trees.

Environment / Climate Change Policies

General Policies

1. Facilitate educational opportunities for the community on climate change and how to reduce greenhouse gas emissions.
2. Draft and adopt a climate action plan or a low carbon resiliency plan to address both mitigation and adaptation strategies and objectives, both for the City as an organization and for the community as a whole. Periodically review and update the plan.
3. Develop a forestry strategy / plan to manage and maintain the community's overall tree canopy, and:
 - Establish goals for tree canopy coverage;

- Mandate tree conservation plans, street tree plantings, and landscaping for all development;
 - Create a mature, interconnected canopy of street trees over time;
 - Increase the total tree canopy cover with the Urban Containment Boundary.
4. Review the BC Building Code Step Code and Zero Step Code requirements and consider exceeding the minimum Provincial requirements to lessen emissions from new buildings.
 5. Consider a building retrofit policy or program with the intent of lowering emissions from existing buildings.
 6. Work with senior governments, the CSRD, First Nations and the agricultural and recreational communities on any invasive species management and mitigation policies or programs.
 7. Consider a landscaping/ xeriscaping policy or development permit area guidelines to address the need for climate resilient plants and water consumption.
 8. Consider amendments to the Zoning Bylaw that would:
 - Provide a density bonus for renewable energy sources;
 - Regulate the use of lease area water surfaces within City boundaries as permitted by legislation (accommodate wildlife (birds) viewing and limited non-motorized watercraft use);
 - Require the installation of EV charging infrastructure in multi family residential dwellings;
 - When environmentally sensitive land is provided to the City for the purpose of environmental protection, allow the owner of the land to use the original site area in calculating density, floor area ratios and minimum lot areas for development or subdivision purposes.
 9. Update Sensitive Habitat and Mapping and Wetland Inventory Mapping (SHIM & WIM) Final Report and mapping
 10. Work with *Columbia Shuswap Invasive Species Society (CSISS)* community groups and other organizations to provide public information and education on invasive species and their impacts.
 11. Work with senior governments, First Nations and the community to reduce the effects of non-point source pollution on source drinking water and recreational water quality.
 12. Work with senior governments, First Nations and the agricultural community to promote groundwater protection.
 13. Work with senior governments, First Nations and the community to improve air quality.
 14. Update the Environmentally Sensitive Riparian Areas Development Permit Area guidelines, to encourage all development and infrastructure projects to conserve environmentally sensitive areas, utilizing alternative development methods, such as clustering, density bonuses, narrowing road rights-of-way, or sharing driveways.

Environmentally Sensitive Areas Policies

1. Applications for new foreshore and water lot leases within the Environmentally Sensitive Lake Areas (Map A-1 Land Use) are discouraged unless they are for conservation or environmental protection and management purposes or represent some other public benefit to the community.
2. The areas designated as Environmentally Sensitive Riparian Areas (Map 5.2 Environmentally Sensitive Riparian Areas) are subject to the Environmentally Sensitive Riparian Areas Development Permit Area guidelines of Section 16 Development Permit Areas.
3. In addition to Development Permit Area requirements, encourage voluntary protection of natural features in cases where it is an objective of the City to protect (for riparian area conservation, water quality protection, or habitat preservation) land in excess of that which is, by virtue of municipal and senior government regulations, required to be protected. Consider the use of tools such as conservation covenants or density bonuses for this purpose.

Community Energy and Greenhouse Gas Policies

1. Continue to implement growth strategies that discourage urban sprawl, encourage healthy communities, and reduce energy consumption.
2. Continue to support active transportation plan implementation through an annual budget allocation similar to other asset management programs.
3. Improve public transportation through a substantial review of the Public Transit services, considering levels of service, frequency and routes in the revised plan.
4. Consider an EV bike subsidy / acquisition program.
5. Improve the energy efficiency and emissions of existing City owned facilities.
6. Improve the energy efficiency and emissions from the City's fleet and vehicles.
7. Encourage and develop policies to support transportation and ride share service options for cars, bikes, and scooters sharing companies.
8. Continue to promote water conservation strategies such as the Water Wise program, incorporation of innovative irrigation technologies, the installation of low flush toilets and low flow aeration faucets, and xeriscaping (i.e. drought resistant landscaping) for all developments.
9. GHG Emission Target Options (*five options are presented here for consideration*)
 - Option 1:
Work toward reducing community wide greenhouse gas emissions levels 50% by 2030, 75% by 2035, and to zero net emissions by 2045, relative to 2019 emissions.
 - Option 2:
Work toward reducing community wide greenhouse gas emissions levels 45% by 2030, 60% by 2035 and carbon neutral by 2050.
 - Option 3:
Work toward reducing community wide greenhouse gas emissions levels 40% by 2030, 60% by 2035, 80% by 2050, relative to 2007 emissions.

Option 4:

Reduce GHG emissions in alignment with the latest values set by the Intergovernmental Panel on Climate Change (IPCC) to limit warming to 1.5°C, or other more stringent science-based metrics.

Option 5:

Create and express GHG emissions reductions targets in the upcoming climate action plan or a low carbon resiliency plan.

10. Continue to measure and publically report on Corporate GHG Emissions.

11. Develop and implement a program to incentivize energy reduction programs in existing residential buildings.

12. Continue conducting energy audits on all major municipal facilities and develop an energy retrofit program for these facilities.

13. Consider expansion of organics collection program to include institutional and commercial land uses.

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Parks, Recreation and Greenways

Parks, Recreation and Greenways Objectives

To provide diverse year-round indoor and outdoor recreation options fostering healthy lifestyles for all residents and visitors through welcoming state-of-the-art and well maintained facilities, accessible parks, as well as connected greenways and natural spaces.

To provide safe, diverse and high quality recreation facilities and programs that are consistent with the population and demographics of the City and region.

To provide a variety of recreation facilities and parks throughout the community and within neighbourhoods that are easily accessible to residents and visitors, to support increased health, wellness and social connections.

To upgrade, maintain and construct the greenways and trail network to improve connectivity, provide all users safe routes throughout the community, and to contribute to a seamless active transportation system.

To preserve a variety of open spaces and natural areas.

Parks, Recreation and Greenways Policies **General Policies**

1. Develop a recreation facility asset management and redevelopment plan (coordinated or as part of an overall facility master plan) including a Recreation Facilities Master Plan.
2. Development of parks, recreation facilities, and greenways shall be in alignment with the Parks and Recreation Master Plan, specific development plans, the Active Transportation Plan and Greenways Strategy, and the future City of Salmon Arm Recreation Facilities Master Plan, as updated from time to time.
3. Seek corporate partnerships and encourage bequests to assist with the planning, acquisition, development, and stewardship of recreation facilities, parks and greenways.
4. Explore methods of securing short and long term funding, such as a parcel tax, for the planning, acquisition, development and stewardship of recreation facilities, parks and greenways.
5. Continue to work in partnership with provincial and federal agencies, non-profit societies, and community groups, on the planning, acquisition, development, and stewardship of recreation facilities, parks and greenways.
6. Encourage the Columbia Shuswap Regional District to examine ways in which the Regional District can complement the parks, recreation and greenways functions in and around Salmon Arm, specifically opportunities for Regional District involvement and assistance in the:
 - a. development of boating facilities (launches, landing sites and parking areas);
 - b. development of major recreation facilities, parks and greenways where the user groups are drawn from the regional population;
 - c. acquisition and development of waterfront properties for public purposes; and
 - d. development of regional active transportation and greenway connections to regional destinations, as well as between Salmon Arm and adjacent communities.

7. Continue using the Joint Partnership Agreement with School District 83 for the funding and development of sports fields and related facilities, as a basis for the establishment of greenways over School District lands, and for use of schools after hours for community programs.
8. Continue to work with the School District to explore opportunities for protecting, retaining, enhancing, and acquiring the open space components of these properties as City parks.
9. Retain the existing Fall Fair Grounds site for a variety of public recreation and community uses.

Indoor Recreation Facilities Policies

1. Continue to recognize, maintain and invest in the major recreation facilities (Rogers' Rink, SASCU Recreation Centre and Indoor Soccer / Multi-Use Arena) and monitor the needs of the community as identified in the Parks and Recreation Master Plan and future Recreation Facilities Master Plan.
2. Expansion, renovation and new development of recreation facilities shall be in alignment with the future Recreation Facilities Master Plan.
3. Develop and improve partnerships with user groups, clubs, and surrounding communities for shared use of major indoor recreation facilities and to support facility development and programming.
4. Continually review the status of facilities, facility programming and user groups to ensure facilities are serving the recreational needs of the community.

Parks Policies

1. Existing and proposed parkland of each type is identified on Map 10.2 Existing and Proposed Parkland.
2. Develop and support the development of existing and proposed parks as shown on Map 10.2 Existing and Proposed Parkland in a phased manner as specified in Parks and Recreation Master Plan and as outlined in the City's Financial Plan.
3. In addition to those identified on Map 10.2 Existing and Proposed Parkland, parks may be developed anywhere in the City.
4. Development of parks shall be in alignment with the Parks and Recreation Master Plan and specific development plans (including the Blackburn Park, Canoe Beach Park, and Klahani Park Plans), as updated from time to time.
5. Consider the City and Provincial gravel pit operations as potential future Parks sites following the phasing out of gravel operations.
6. Ensure outdoor recreation facilities and parks are supported with adequate access, storage and sanitary facilities.
7. Consider adopting a policy (potentially within the future Recreation Facilities Master Plan) addressing the storage of sports equipment at City fields related to the use of shipping containers.

Greenways Policies

1. Existing and proposed greenways are identified on Map 10.1 Existing and Proposed Greenways.
2. Develop and support the development of existing and proposed greenways as shown on Map 10.1

Existing and Proposed Greenways in a phased manner as specified in the Greenways Strategy, Active Transportation Plan, and as outlined in the City's Long Range Financial Plan.

3. In addition to those identified on Map 10.1 Existing and Proposed Greenways, greenways may be developed anywhere in the City.
4. Use the Subdivision and Development Servicing Bylaw to include greenway dedication and ensure construction standards are met as a condition of subdivision or development approval
5. Work with and support community organizations and volunteers, including the Shuswap Trail Alliance, in implementing the Greenways Strategy, and Active Transportation Plan including: planning, acquisition, development and stewardship of greenways; infrastructure development; communication about the greenways network; and ongoing maintenance.
6. Develop and support the development of an active transportation network that provides connectivity between greenways, including multi-use paths, sidewalks, on-road bike routes and roadside corridor greenways that accommodate all users.

Parks and Greenways Acquisition and Development Policies

1. Acquire parks at the time of subdivision (including bareland strata subdivisions) in accordance with the provisions of the *Local Government Act*, comprising five percent of the subdivided land or an equivalent cash in lieu. The criteria for selection of the parkland should include the following considerations, among others:
 - a. Whether the subdivision is in proximity to an existing park or linear greenway corridor and is suitable for expansion;
 - b. Whether the subdivision is in proximity to a future park or linear greenway corridor as indicated on Map 10.2 Existing and Proposed Parkland;
 - c. Whether the land dedicated as park is of a reasonable size and has characteristics suitable for park or greenway development;
 - d. Park dedication that occurs in conjunction with storm detention ponds or facilities does not contribute to the required 5% parkland dedication; and
 - e. Park dedication for Greenway may be dedicated as Road or Park and does not contribute toward the 5% required Parkland dedication.
2. For multi-phase developments, park dedication and/or development should occur in the first phases of development.
3. Support the protection of natural areas through the considered acquisition of environmentally sensitive areas as parks, including ravines, bluffs, riparian areas, habitat, and steep slopes, as important natural areas.
4. Require, at the discretion of City Council, land to be dedicated and paths, trails and roadside corridors to be constructed for greenways and active transportation corridors as a consideration for the approval of rezoning applications. Refer to Map 10.1 Existing and Proposed Greenways and the Greenways Strategy and Active Transportation Plan as a guide for determining the approximate locations for future greenways and the standards of greenway construction.
5. Require, at the discretion of the Approving Officer, land to be dedicated and paths, trails and roadside corridors to be constructed for greenways as a consideration for the approval of subdivision applications. Refer to Map 10.1 Existing and Proposed Greenways, the Greenways Strategy and

Active Transportation Plan as guides for determining the approximate locations for future corridors and the standards of construction.

6. Encourage/require greenway connections at the end of cul-de-sacs and in combination with emergency access routes to enhance connectivity between neighbourhoods wherever feasible.

7. Explore other methods for acquiring land for parks and greenways through the development process, such as density bonus, purchase, negotiation, donation and statutory right-of-way.

Lake Recreation Policies

1. Areas designated for Lake Recreation are identified on Map A-1.

2. The community marina and boat launches are shown on Map 10.3 Community Facilities.

3. Within the areas designated Lake Recreation, the water surface may accommodate all forms of water-oriented recreational activities, including marinas and marina facilities.

4. Explore locations for future public Lake access points (motorized and non-motorized).

5. The upland areas designated Lake Recreation may accommodate commercial and institutional uses, such as marinas, that support water-oriented recreational activities.

6. Discourage the construction of new or modified marinas and other marine facilities along Shuswap Lake unless they mitigate negative impacts on the lake, the natural riparian habitat, or the visual character of the shoreline, and they provide opportunities for public access to and along the shoreline.

7. Consider Developing guidelines or Development Permit Area to regulate the construction of new or modified marinas or other marine facilities on Shuswap Lake

Forest Reserve Policies

1. Forestry uses are permitted in areas designated as Forest Reserve subject to approval by the appropriate federal and provincial agencies. Forestry uses include silviculture, watersheds, timber extraction, compatible ranching and/or recreational activities.

2. Work with the appropriate federal and provincial agencies to preserve and manage critical scenic views and watersheds within the Forest Reserve area to protect important aesthetic values and essential timber and water resources.

Arts, Culture, Heritage, Social, and Community Services

Arts, Culture, Heritage, Social, and Community Services Objectives

To create inclusive spaces and places throughout the City.

To ensure residents of all ages, abilities, and backgrounds are represented in the community and can create, experience, and contribute to arts and culture within the City.

To ensure residents and visitors encounter art and culture in public places throughout the City as part of their daily experiences.

To design and program public spaces for public art, social interaction, and special cultural events.

To promote programs that celebrate local arts and culture and celebrate history and heritage.

To encourage and support community events, performances, and gathering spaces.

To increase education and awareness of different cultures.

To facilitate partnerships to improve and create new venues for arts and culture, entertainment and performance.

To promote the installation of public art in public and private developments.

To promote the preservation of community heritage.

To promote and support new activities to do in the evenings and activities for all ages.

To encourage age friendly opportunities that support young families and seniors.

To work with, support, and consult government agencies, community organizations and volunteer groups that provide diverse community and social services.

To encourage more efficient utilization of existing institutional lands.

To provide emergency and protective services consistent with the City's population and demographics.

To support health and education services consistent with the needs in the City and region.

To ensure all public facilities (recreational, municipal, etc.) are accessible to all ages, ethnicities, incomes and abilities.

To explore and implement opportunities to encourage and improve public participation by diverse members of the community in planning processes and community organizations.

Arts, Culture, Heritage, Social, and Community Services Policies

General Policies

1. Map A-1 Land Use designates lands for institutional use and 10.3 Community Facilities identifies the locations of community facilities such as:

- City Hall
- Shuswap Regional Airport
- Shuswap Lake General Hospital and other health care facilities
- Salmon Arm Art Gallery
- R. J. Haney Heritage Village and Museum
- Public schools
- Recreation facilities and community centres
- Library
- Places of worship
- Cemeteries
- Okanagan College
- City Public Works Yard
- Fire halls
- Waste Water Treatment Plant, Water Pollution Control Centre, and regional landfill
- RCMP detachment
- Shelters

2. Acknowledge the role of community groups in providing arts, culture, heritage, and community services, and work in collaboration with these groups, particularly in terms of supporting volunteer organizations and encouraging shared facilities.

3. Continue to communicate and work cooperatively with community service agencies such as local service clubs, non-profit organizations, places of worship, the CSRD, educational institutions, the library, health care institutions, and provincial and federal agencies to promote a healthy community, to plan future facilities, to ensure a full spectrum of services, and to identify and work cooperatively to address emerging issues.

4. Consider accessory commercial and multi-unit residential developments on Institutional properties where this will not prevent or hinder the primary Institutional use.

5. Encourage major Institutional uses that serve the entire community, such as health care facilities, government buildings, and cultural facilities, to locate within the Downtown.

6. Encourage civic buildings to be located on the frontage of public squares, major streets or similar high profile locations with good visibility and accessibility and where buildings complement a development node (e.g. other civic, recreation, commercial, education, and/or cultural activities).

7. Develop a place naming policy for parks, buildings, and streets that includes community engagement and culturally appropriate guidelines.

Arts and Culture Policies

1. Update the Arts and Culture Master Plan periodically and work towards implementing the objectives identified in the plan.

2. Explore opportunities to establish an arts and cultural precinct in proximity to the existing theatres and art gallery.

3. Facilitate partnerships to build a Community Arts Centre, potentially including visual and performing arts, with capacity for a variety of programs and events.
4. Update the Performing Arts Theatre and expand public art spaces and venues Downtown.
5. Work with community partners to expand arts and culture programs that reflect the community's diversity.
6. Encourage public art in Downtown, in major public spaces, and on large development projects.
7. Partner with cultural groups and organizations to increase the visibility and recognition of the many cultures and traditions within the City and region.
8. Support initiatives and programs to increase education and awareness of different cultures.
9. Continue to support festivals and events that celebrate diversity (e.g. Gathering Together Festival, Pride Festival).
10. Encourage festivals throughout the year, beyond the summer months.

Heritage Preservation and Conservation Policies

1. Promote awareness of the City's heritage resources by implementing the recommendations of the Heritage Strategy, including additions to the Heritage Register.
2. Support the Community Heritage Commission with reoccurring updates to the Heritage Strategy.
3. Encourage the protection of the integrity of the sites and buildings on the Heritage Register.
4. Develop guidelines for alternations for buildings on the Heritage Register.
5. Develop management plans for City owned heritage buildings and properties.
6. Continue to support the three designated municipal heritage sites in the City: R.J. Haney House, Salmon Arm Art Gallery, and the Old Court House facade.
7. Encourage the sensitive adaptation of heritage buildings for other uses.
8. Encourage new development to respect buildings, sites, and features with heritage significance, even when those features are close to rather than within the development site.
9. Support the addition of more streets into the Historic Street Names Program.
10. Support the R.J. Haney Heritage Village and Museum in preserving an important representation of the City's heritage.
11. Integrate heritage resources into economic development and tourism strategies.
12. Support educational opportunities and events that reflect the community's diverse history.

Community Services Policies

1. Institutional uses, including schools, places of worship, health care facilities, libraries and provincial and federal offices are only supported within the Urban Containment Boundary.
2. Official Community Plan Amendment applications are not required to locate Institutional uses within the Urban Containment Boundary. Public input regarding locations may be received through the Public Hearing process associated with rezoning applications for new institutional uses.
3. Encourage Institutional uses to locate within convenient access of major transportation routes to ensure ease of accessibility and to minimize negative impacts on residential developments.
4. Ensure adequate space is designated in the growth strategy to accommodate health services in the community, ensuring convenient access for residents.
5. Support the creation of community spaces which are safe and support connection, particularly for marginalized groups (e.g. youth).
5. Work with the Shuswap Recreation Society on a recreational access strategy or plan to support access to low-barrier recreation and community programs.

Social Services Policies

1. Community support services and uses, such as shelters, transition/youth homes, daycares and other forms of social housing, are only supported within the Urban Containment Boundary.
2. Official Community Plan amendment applications are not required to locate community support services and uses within the Urban Containment Boundary. Public input regarding locations may be received through the public hearing process associated with any rezoning applications.
3. Acknowledge the role of federal and provincial levels of government and non-profit sectors as the main providers of social programs, facilities and services within the City, and work in a cooperative and supportive capacity with these service providers.
4. Clearly communicate and articulate the City's role in social development as being one of a facilitator and enabler, and not one of direct service provision.
5. Work with the Province and community partners to expand social wellbeing programs.
6. Support the development and expansion of programs for people dealing with mental health and substance use issues, housing insecurity, and job insecurity, especially Indigenous peoples and newcomers.
7. Develop an Anti-Racism Strategy in conjunction with community partners.
8. Support social planning through the following measures:
 - Liaise with community groups, not-for-profit organizations, and neighbourhoods;
 - Liaise with First Nations;

- Encourage increased efforts to engage with and support those with specific needs, e.g. children, youth, families, seniors, First Nations, and people with disabilities;
- Encourage and facilitate child care facilities and services, e.g. in new developments, places of employment, education, and cultural facilities;
- Continue to expand and improve inclusive and affordable recreation, arts, and culture programs for all age groups;
- Assist groups in acquiring grants or other types of funding from senior levels of government or other sources for social needs by providing the information that is necessary and assisting with contact information;
- Work with regional partners, encourage social issues to be considered in new development proposals where appropriate, recognizing that social issues may include affordable and accessible housing; daycare; transit; access to schools, recreation and government services; healthy, safe and violence-free communities; and
- Explain and support social issues during the review and presentation of development proposals.

Community Protection Policies

1. Continue to provide and maintain police protection service levels in accordance with the growing and changing needs of the community, working with the RCMP to ensure rational costs and efficient service delivery.
2. Continue to provide and maintain bylaw enforcement service levels in accordance with the growing and changing needs of the community.
3. Utilize Crime Prevention Through Environmental Design's three basic strategies – natural access control, natural surveillance, and territorial reinforcement – to address the security, safety, and well-being of residents.

School and Childcare Policies

1. Work with School District No. 83 in the planning of new school sites to meet future needs.
2. Work with School District No. 83 in planning for future land use options for lands which may become surplus to the School District's needs.
3. Support and work with School District No. 83 in the development of a School Site Acquisition Costs program.
4. Encourage the joint development of neighbourhood parks, community recreation, and school sites.
5. Continue using the Joint Partnership Agreement with School District No. 83 for the funding and development of sports fields and related facilities, and for the use of schools for after-hour community programs.
6. Explore the idea of daycare as a use in all land use designations to support more childcare facilities.

Economic Development

Objectives

To maintain and improve community affordability.

To maintain low unemployment levels.

To support sustainable economic development, including low-emission industries and clean technology.

To assist in creating a diverse economy and economic base.

To help create stable employment opportunities and earn a living wage.

To ensure that jobs are well connected to housing by foot, bike, and transit.

Policies

1. Continue to support a strong economic base by:

- high level planning for the servicing of new land associated with business and job creation (e.g. agricultural processing and industrial/manufacturing);
- supporting the efforts of the Salmon Arm Economic Development Society;
- striving to protect industrial and commercial lands for future industrial and commercial uses;
- striving to protect the natural environment;
- encouraging environmentally friendly industries, green infrastructure;
- supporting the protection of heritage resources;
- maintaining the Urban Containment Boundary, supporting development within and limited growth and development outside of the Urban Containment Boundary.

2. Continue to support and partner with the Salmon Arm Economic Development Society (SAEDS) as part of providing economic development services to the community.

3. Continue to support SAEDS to manage any Municipal and Regional District Tax (MRDT) program to disburse these funds for local economic development initiatives.

4. Continue to support the tourism industry and tourism development programs.

5. Protect commercially zoned lands from redesignation or rezoning to residential uses, unless other significant priorities or objectives are achieved.

6. Protect industrially zoned lands from redesignation or rezoning to residential uses, unless other significant priorities or objectives are achieved.

7. Through the rewrite of the Zoning Bylaw, seek to

- expand opportunities for commercial uses (e.g. convenience stores, coffee shops, and restaurants in neighbourhood) particularly in the Canoe, Hillcrest and Raven areas;
- the City will expand opportunities to allow live-work options in both commercial and residential areas.

8. In any review of transit services, the City will seek to improve connectivity especially transit connections to provide enhanced employee access to industrial areas.

9. Work with SAEDS to support programs aimed at increasing workforce housing and employee skills and training opportunities.
10. Work with SAEDS to support programs aimed at circular economy principles and programs.
11. Support SAEDS in promoting industrial businesses that are complementary to and utilize the strengths of the community.
12. Support the SAEDS in promoting commercial businesses that are complementary to and utilize the strengths of the community.

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Transportation

Transportation Objectives

To provide a variety of transportation options for residents and visitors that are safe, efficient, and less carbon intensive.

To improve walking and cycling opportunities by improving sidewalks and bike routes associated with the road network.

To expand the route network and frequency of public transit.

To support improved services at the Shuswap Regional Airport (Salmon Arm).

To ensure necessary parking is available in the City while minimizing its impact on the urban character.

Transportation Policies

General Policies

1. Develop, improve and maintain a suitable transportation network that supports the various functions that are intended, while:

- maintaining sufficient flexibility to respond to changing needs;
- ensuring the provision and safety of all modes of transportation;
- encouraging reduced reliance on private automobiles;
- encouraging increased use of walking, cycling and transit, through the construction of new infrastructure; and
- supporting policies respecting development of a compact community with complete neighbourhoods providing local services.

Active Transportation General Policies

1. Continue to encourage and support active transportation through implementation of the Active Transportation Network (ATN) Plan.

2. Focus on key priority connections established by the ATN Plan based on high demand and high need areas that either currently experience, or have the potential for generating, the highest levels of active trips. (e.g. uptown to downtown connection, connections to schools).

3. Prioritize the connections to key destinations including schools, employment centres, parks, shopping centres, healthcare, recreation sites, places of worship, and municipal buildings.

4. Require adequate lighting along active transportation routes taking into consideration different levels of lighting to enhance safety.

5. Encourage multi-use pathways in priority active transportation corridors to serve all active transportation users.

6. Ensure year round maintenance of active transportation infrastructure.

7. Identify funding sources for the installation of active transportation infrastructure.

8. Partner with School District No. 83 to promote walking or rolling to school by identifying safe routes to schools.

Pedestrian Policies

1. For subdivision and development purposes and capital works planning, sidewalks should be required as follows:

- along the frontages of commercial, multi-family residential, and institutional parcels;
- along both sides of arterial and collector roads; and
- along one side of local roads in low density residential areas.

2. Encourage the separation of sidewalks from the vehicle portion of roads with grass boulevards and street trees, where space, utilities, and topography permit.

3. Ensure sidewalk and multi-use path design incorporates accessible design elements. Require trail, sidewalk, or multi-use path connections between neighbourhoods through the subdivision process to create pedestrian connectivity and neighbourhood linkages.

4. Fill gaps in the pedestrian network based on priority.

5. Explore opportunities to create pedestrian-only streets, temporarily, seasonally, or permanently.

6. Enhance the pedestrian experience along pedestrian centered streets and in places of cultural significance.

Bicycle and Rolling Policies

1. Continue to enhance the safety of cyclists by improving cycling infrastructure, including the construction of bike lanes along the bike routes identified in the ATN Plan and on Map 13.1 Cycle Network Plan.

2. Separate bike lanes through barriers such as planter boxes, concrete barriers, flexible delineator posts, or grade changes.

3. Develop a complete and connected cycling network for people of all ages and abilities that connects to key destinations.

4. Support multi-modal trips by providing bike racks on buses and bike parking at bus stops.

5. Consider amendments to the Zoning Bylaw that:

- Improve the safety of users by physically separating bicycle and rolling lanes from vehicular traffic destinations (as identified in ATN Plan or Map 13.1 or Map 10.1) in Salmon Arm based on priority.
- Encourage end of trip facilities such as showers, lockers, ebike and scooter charging stations, bike storage, bike repair stations, and bike lock ups.
- Provide bike lock-up facilities in key destinations such as downtown, parks and beaches, and at festivals.
- Identify bicycle parking requirements.

6. Consider amendments to the Development Permit Process that will encourage bicycle parking and storage in new multi-family and mixed use developments.

Transit Policies

1. Expand the route network and frequency of transit as required to service demand.
2. Conduct a transit study and develop a plan for future transit expansion.
3. Work with the Columbia Shuswap Regional District to support enhanced regional transit options.
4. Consider transit infrastructure and the feasibility of transit service delivery during the subdivision process.
5. Work with BC Transit to improve the experience of and convenience of using transit, by offering multiple payment methods.
6. Work with School District No. 83 to ensure routes and schedules coordinate with and meet the needs of students.
7. Ensure that the transit network is taken into account in the planning and design of Arterial and Collector Roads.
8. Coincide public transit routing and service frequency with the sequence of development in the City to address the needs of higher concentrated populations and to achieve the most cost-effective operation.
9. Increase HandyDART service levels.
10. Encourage transit use by ensuring that good pedestrian infrastructure, bike parking, and benches are located near transit stops.
11. Ensure transit stops are accessible by a variety of other transportation modes and by people of varying mobility levels.
12. Identify other funding sources for public transit service delivery.

Vehicular Policies

1. The Road Network Plan shown on Map 13.2 was developed to service current and future growth areas, recognizing provincial highway functions. The Road Network Plan consists of the following components:

Provincial Highways;
Arterial Roads;
Collector Roads; and
Local Roads.

Advanced road network plans for Growth Areas A, B, and C – with maps are under development.

2. Develop a Complete Streets program to create (future) road network plans.
3. Review existing truck routes and explore alternative routes.
4. Update a City wide Transportation Master Plan which should include a review of:

- traffic patterns;
- projections; and
- potential impacts along the road network, including safety for non-vehicular modes of transportation and potential mitigation strategies.

5. The Subdivision and Development Servicing Bylaw may identify exceptions to the road and highway right-of-way widths, including reductions or increases in certain locations or circumstances.

6. Improve road safety in rural areas by encouraging lower speeds through traffic calming infrastructure.

7. Ensure right-of-way widths for roads in designated areas include space for active transportation infrastructure.

8. Prepare policies and procedures for funding the acquisition of additional lands required to accommodate 25 metre rights-of-way for arterial roads.

9. All roads not designated as provincial highways, arterial roads, or collector roads on Map 13.2 Road Network Plan are classified as local roads.

Provincial Highway Policies

1. The designated Provincial Highways shown on Map 13.2 Road Network Plan are the Trans Canada Highway and Highway 97B.

2. Work with the Ministry of Transportation and Infrastructure (MOTI) to carefully consider subdivision or changes in land use designations which require new access requirements along Provincial Highways.

3. Development may be permitted in the Highway Service/Tourist Commercial and City Centre designations along the Trans Canada Highway and Highway 97B corridors in accordance with this plan prior to the development of frontage or service roads, provided that sufficient land area is reserved for same and restrictive covenants are registered on title that will prohibit direct highway access once those roads are constructed.

4. Work with MOTI to undertake immediate planning to establish the concept and right-of-way requirements for frontage or service roads.

5. Work with MOTI to improve the Provincial Highways as part of the City's long-term road network as follows:

- plan for an additional intersection or improvements on Highway 97B for the Salmon Arm Industrial Park;
- plan for intersection improvements at Highway 97B and 10 Avenue SE; and
- plan for intersection improvements on the Trans Canada Highway at 50 Street NE in Canoe.

6. Work with MOTI to minimize the negative impacts of the Trans Canada Highway on the community by improving safety and crossing opportunities for all modes of transportation, and improving maintenance along the highway right-of-way.

Parking and Loading Policies

1. Review parking requirements in the Zoning Bylaw and the Downtown Specified Parking Area to ensure a sufficient parking supply while minimizing impacts on urban character.
2. Create a balanced parking strategy to support businesses while also encouraging alternate transportation modes.
3. Consider the feasibility of a parking structure/parkade downtown to free up commercial land currently used for surface parking.
4. Ensure adequate parking and loading zones for goods delivery in commercial areas both on-street and through the Development Permit process.

Rail System Policies

1. Work with Canadian Pacific Kansas City (CPKC) Rail to explore means of addressing noise, safety and crossing opportunities for all modes of transportation, emergency situations, the transportation of hazardous goods, and the visual impacts of the railway right-of-way on the community.
2. Work with CPKC Rail to develop a grade-separated cyclist and pedestrian crossing of the rail line between the Growth Area C (Map 4.1) and the foreshore trail in the vicinity of the Raven neighbourhood.

Air Transport Policies

1. Figure 1 delineates the Shuswap Regional Airport (Salmon Arm) operational area and approach corridor.

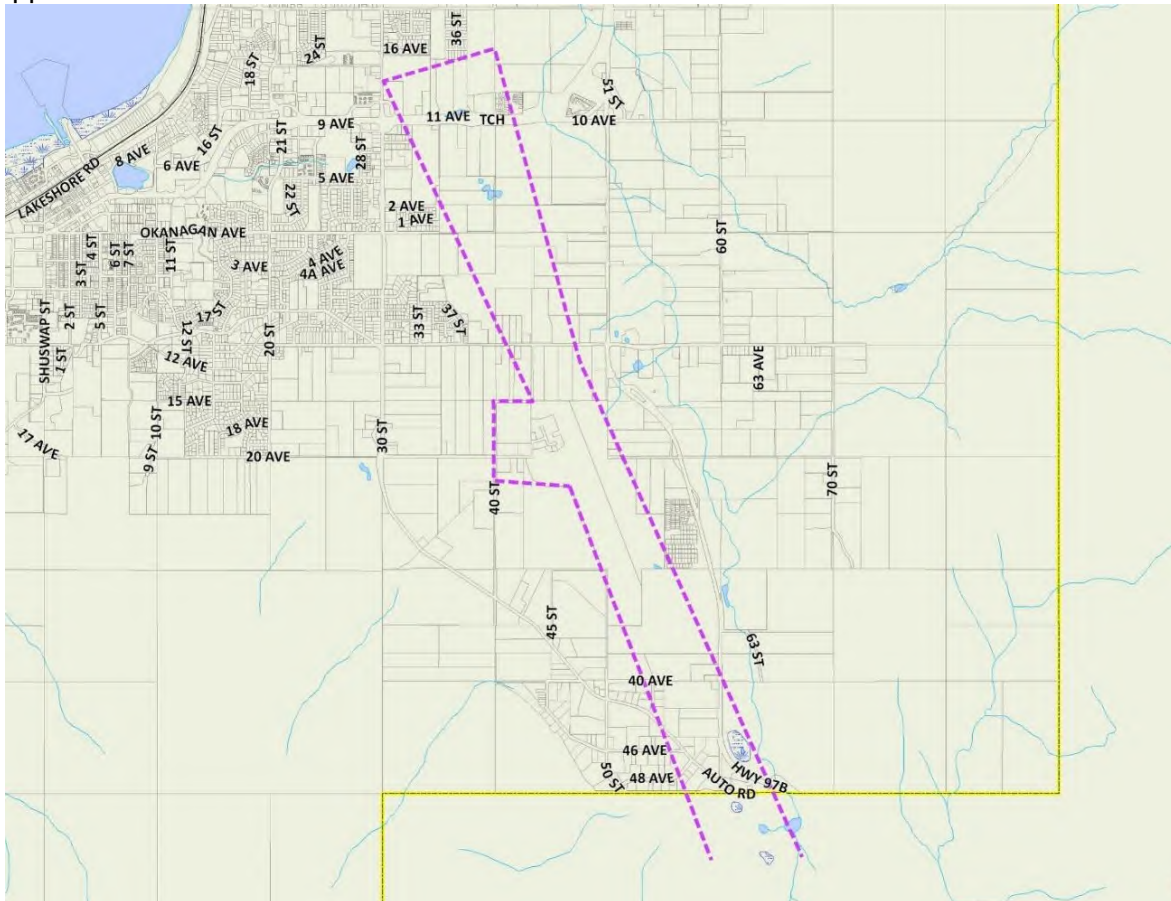


Figure 1: Shuswap Regional Airport (Salmon Arm) Operational Area and Approach Corridor

2. Protect the airport approach corridor for air operations by implementing the following land use criteria through a Zoning Bylaw Overlay:

- restrict the height of buildings and structures through zoning controls or aerial easements;
- restrict land uses which would be adversely impacted by excessive aircraft noise or contribute to conflicts with air operations;
- implement land use controls to minimize problems of electrical interference (transmission towers, etc.), trees (e.g. height), and bird hazard (landfills, etc.); and
- comply with operational restrictions of senior government agencies (e.g. Canadian Aviation Regulation).

3. Support the concept of one common use facility at the Shuswap Regional Airport (Salmon Arm) to discourage ad hoc, unlicensed facilities. Emergency helipads may be permitted elsewhere, however these are not of a general commercial nature but primarily an intermittent emergency activity to facilitate movement of patients and medical personnel.

4. Helipads may be permitted as an accessory use in conjunction with commercial or industrial operations subject to such facilities having no provision for on-site fueling, maintenance or storage of aircraft.

5. Continue to recognize and support the Shuswap Regional Airport (Salmon Arm) as an important transportation asset.

Water Transport Policies

1. Areas designated for Lake Recreation on Map A-1 Land Use may accommodate all forms of water-orientated activities including marinas and marine facilities.

2. Where adjoining the General Industrial designation in Canoe, Lake Recreation areas may accommodate water access and other activities ancillary to the industrial activity (e.g., log transport and storage).

3. Floatplane operations and support facilities may be allowed in conjunction with the City's Marine Park wharf and marina facility, provided mitigation measures are taken to address potential disturbance to environmental resources of the foreshore.

4. Restrict floatplanes to landing in the Lake Recreation designation area and taxiing to the wharf to address potential disturbance to environmental resources of the foreshore.

5. Explore the feasibility of inter community water taxis and services between Canoe and the Wharf and between Canoe and Herald Provincial Park.

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Utilities and Infrastructure

Utilities and Infrastructure Objectives

To develop and manage utilities in a manner that emphasizes energy conservation, environmental sustainability, and fiscal responsibility.

To manage the City's infrastructure systems consistent with best practices.

To improve the City's stormwater system based on best practices to improve water quality and quantity in watercourses and Shuswap Lake.

To integrate green infrastructure into storm water management infrastructure and protect ecosystem function.

To improve the City's management of solid waste in partnership with the CSRD.

Utilities and Infrastructure Policies

General Servicing Strategy Policies

1. Continue to provide water, sanitary sewer, and storm drainage utilities consistent with the phasing of Growth Areas A, B, C and E. (see Map 4.1).
2. Continue to carry out an infrastructure analyses through capital asset management and use this information to identify priorities for upgrading and improving the system.
3. Manage water, sanitary sewer, and storm drainage utilities in a manner that emphasizes energy conservation, environmental sustainability and fiscal responsibility. Consider long term maintenance programs and cost effective operation of existing and future services in relation to financial sustainability when planning new utilities.
4. Continue to develop demand management strategies for water (e.g. reducing water use through metering, conservation measures, low water-use fixtures and appliances).
5. Upgrade or require upgrading of services in existing serviced areas to accommodate cost effective redevelopment to higher densities. Priority setting and evaluation will occur primarily through formal infrastructure planning and the capital budget process.
6. Municipal utilities in the Forest Reserve, Salmon Valley Agriculture and Acreage Reserve zoning designations should not exceed the existing standard or be extended, except for the municipal water system outlined in Section 6 Rural & Agriculture.
7. Review the Development Cost Charge Bylaw on a regular basis to ensure the changes are appropriate and consistent with the Long Term Financial Plan.
8. Continue to use the Subdivision and Development Servicing Bylaw to identify the works and services (e.g., roads, water, sanitary, storm drainage, sidewalks, street lights) required at the time of subdivision, building permit or development approval.
9. Continue to communicate and work cooperatively with other private utility providers within the City, including annual capital planning consultations, towards the provision of a full range of efficient, effective, modern services to resident subscribers.

10. Upgrading of local infrastructure beyond current service levels may be considered through use of local service area provisions where the project is funded primarily by benefiting property owners.

Water Policies

1. Support extensions of the water system within the Urban Containment Boundary consistent with the phasing of Growth Areas A, B, C and E.
2. Consider extensions of the water system to existing developments outside the Urban Containment Boundary when the project is funded primarily by the benefiting property owners.
3. The City will not support the establishment of new private community water systems within the City.
4. The City will not assume responsibility for the maintenance and operation of any private community water systems already operating within the City.
5. Prepare and implement water source protection strategies for the City's potable water supply sources as follows:
6. Work in cooperation with other watershed stakeholders to implement the recommendations of the Water Source Protection Plan for the East Canoe Creek watershed as shown on Map 14.3 Water System.
7. Complete a water source protection strategy for the City's Shuswap Lake potable water source and where appropriate work with other Shuswap Lake watershed stakeholders, including the Shuswap Lake Integrated Planning Process.
8. Address deficiencies in fire flow capabilities (insufficient volume or pressure for firefighting) within the water system at the time of subdivision or development approval.
9. Update and implement a comprehensive water conservation strategy, including outdoor water use restrictions, water metering options and conservation based water rates.

Sanitary Sewer Policies

1. Support extensions of the sanitary sewer system within the Urban Containment Boundary (UCB) consistent with the phasing of Growth Areas A, B, C and E.
2. The City will not support the extension of the sanitary sewer system outside the UCB. Private sanitary sewer connections outside the UCB may be considered for existing developments where main extensions are not required, subject to Council approval.
3. Continue to manage the City's sanitary sewer system consistent with best practices and the City's Liquid Waste Management Plan.
4. Outside the Urban Containment Boundary, septic tank and ground disposal will continue to be the primary method of disposing of sewage effluent, subject to the regulations of the Interior Health Authority and Ministry of Environment. Ground disposal or satellite wastewater treatment plants may be considered for developments within the Urban Containment Boundary when connections cannot be feasibly made to the City system, and groundwater resources can be protected.

Storm Drainage Policies

1. Periodically review and update the City's Integrated Stormwater Management Plan, including recommendations on best practices for managing rainwater and stormwater to protect and enhance water quality and quantity in watercourses.
2. Encourage stormwater utilities to be designed and constructed in an environmentally sensitive manner using best management practices (e.g. with permeable absorbent landscapes, natural filtration of water using vegetation, slowing runoff rates). Communicate these best management practices to the public and to developers.
3. Require new development to address rainwater and stormwater management, ensuring that post-development flows off the site do not exceed pre-development flows.
4. Continue to use the existing natural drainage pattern as the primary storm drainage system and use stormwater detention/retention as the principal means of meeting the objective of maintaining post-development flows at pre-development levels.
5. Require new development to undertake on-site siltation control measures where runoff could enter the stormwater system or could damage nearby ecosystems.

Green Infrastructure Policies

1. Utilize the ecological services of green infrastructure (such as rainwater capture and water quality treatment) wherever possible.
2. Conduct an inventory of green infrastructure assets in the community.
3. Review the Subdivision and Development Control Bylaw to include green infrastructure provisions.
4. Conduct an inventory of the City's storm water ditches to determine purpose, function and the appropriate level of service.

Solid Waste Policies

1. Continue to use the landfill in the City and other infrastructure managed by the Columbia Shuswap Regional District for all solid waste, recycling and organics programs.
2. Ensure that an adequate buffer is maintained around the landfill and that appropriate land uses are allowed in the buffer, to ensure minimal impact to landfill operations.
3. Continue to work cooperatively with the Columbia Shuswap Regional District regarding operation and management of the landfill, and the curbside garbage, organics and recycling pick-up programs.
4. Continue to work cooperatively with the Columbia Shuswap Regional District regarding implementation of any recommendations of an updated Solid Waste Management Plan, and any waste prevention and diversion strategies.
5. Continue with and enhance the residential recycling collection program.
6. Support the development of proposed new recycling collection facilities in industrial areas.
7. Continue and review and enhance the residential organics collection program to divert more materials from the landfill, working with commercial operators and collections from multi family and

commercial properties.

8. Consider developing policy regarding the diversion of construction and demolition waste from the waste stream.

9. Develop policy around solid waste and recycling collection facilities in the multi family residential Development Permit Area Guidelines.

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Potential Hazard Areas

Potential Hazard Areas Objectives

To protect human life and property from potential hazards.

To enhance community safety and resiliency from climate related hazards.

To minimize human activities and development that increase hazards.

To plan for potential hazards associated with climate change.

To direct development away from areas subject to hazards.

Potential Hazard Areas Policies

1. The floodplain is identified as the 1:200 year floodplain and is shown on Map 15.1 Potential Hazard Areas.

2. The floodplain is subject to the Potential Hazardous Development Permit Area guidelines of Section 16 Development Permit Areas.

3. Steep slopes are identified as slopes over 30% as shown on Map 15.1 Potential Hazard Areas. Steep slopes are subject to the Potential Hazardous Development Permit Area guidelines of Section 16 Development Permit Areas.

4. Review and update the Hazardous Areas Development Permit Area guidelines and other development permit area guidelines, particularly to consider:

- hazards related to wildfire;
- creating a possible Wildfire Interface Development Permit Area; and
- creating FireSmart BC aligned landscaping standards.

5. Review and update the floodplain provisions of the Zoning Bylaw.

6. Consider options for regulating the removal and deposition of soils and other fill materials.

7. To mitigate the risks associated with wildfire hazards, lands outside the Urban Containment Boundary (Map 4.1 Urban Containment Boundary), will be required, as a condition of rezoning or subdivision approval, to register a *Land Title Act* s. 219 restrictive covenant on title. The covenant shall notify land owners that the land may be at the risk of wildfire, and that owners should take appropriate precautions in accordance with FireSmart BC principles. The covenants shall also save the City harmless in the event of a wildfire.

8. Continue to encourage property owners to follow FireSmart BC principles to protect their property, including conducting assessments and possible incentives.

9. Periodically review and update its' Community Wildfire Protection (or preparedness) Plan.

10. Continue and expand fire smart initiatives and wildfire related educational programming through the Fire Department.

11. Create an annual line item / amount in the budget for wildfire management purposes and

projects.

12. Continue to work with the Columbia Shuswap Regional District and the Shuswap Emergency Program in preparing and responding to hazards and emergencies in the City and the region, and should participate in any multi-jurisdictional emergency management organizations if established.

13. Work with the Columbia Shuswap Regional District and the Shuswap Emergency Program in the preparation of any:

- Risk assessment of hazards;
- Emergency management plan(s); and
- Emergency evacuation plan(s) (particularly for vulnerable populations)
- Establishing areas for emergency response and rapid deployment

14. Continue to support Rap Attack services adjacent to the airport.

15. Consider the use of municipal facilities in case of emergencies such as cooling and warming centres.

16. When property located in areas of established wildfire risk is transferred to the City, the City should ensure that any wildfire fuel modification is completed prior to the transfer being completed.

17. Complete a corporate business continuity plan for the City.

18. Restrict development on steep slopes over 30%, and ensure that these areas are retained as public or private natural open space.

19. Consider incorporating the Guidelines for New Development in Proximity to Railway Operations, prepared by the Federation of Canadian Municipalities into the relevant provisions of the Zoning Bylaw or Development Permit Area Guidelines.

20. Develop a steep slopes development permit area or create steep slopes regulations within the Subdivision and Development Servicing Bylaw to ensure appropriate servicing standards on steep slopes, and to:

- ensure slope stability
- address geotechnical concerns
- retain significant tree cover (recognizing the challenges of retaining patches of trees on steep slopes)
- encourage tree replacement planting
- minimize cut, fill and the need for retaining walls

DEVELOPMENT PERMIT AREA GUIDELINES

- A. Environmentally Sensitive Riparian Areas Development Permit Area
- B. Multi Family Residential Development Permit Area
- C. Infill Residential Development Permit Area
- D. Downtown Commercial Development Permit Area
- E. Highway Service/Tourist Commercial and Neighbourhood Commercial Development Permit Areas
- F. Industrial Development Permit Area
- G. Potential Hazard Areas Development Permit Area
- H. Farm Protection Development Permit Area

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A. Environmentally Sensitive Riparian Areas Development Permit Area

Designation

Pursuant to Section 488(1)(f) of the *Local Government Act*, all Environmentally Sensitive Riparian Areas shown on Map XX Environmentally Sensitive Riparian Areas are designated Environmentally Sensitive Riparian Areas Development Permit Areas. These Development Permit Areas affect all parcels of land adjacent to or containing a watercourse or stream as defined in the Riparian Areas Protection Regulation. The Environmentally Sensitive Riparian Areas Development Permit Area is equivalent to the riparian assessment area of the Riparian Areas Regulation RAPR, and means:

- a) for a stream, the 30 metre strip on both sides of the stream measured from the high water mark;
- b) for a ravine less than 60 metres wide, a strip on both sides of the stream measured from the high watermark to a point that is 30 metres beyond the top of the ravine bank; and
- c) for a ravine 60 metres wide or greater, a strip on both sides of the stream measured from the high water mark to a point that is 10 metres beyond the top of the ravine bank.

Objectives

To protect Environmentally Sensitive Riparian Areas that, through their protection, will help in meeting the following objectives:

- a) protection of environmentally sensitive areas;
- b) prevention or reduction of air, land, and water pollution;
- c) protection of quality and quantity of groundwater and surface water;
- d) conservation of scarce resources; and
- e) protection of fish and fish habitats.

Application

Unless a Development Permit waiver has been issued, all properties shown as Environmentally Sensitive Riparian Areas on Map 9.1 Environmentally Sensitive Riparian Areas will require a Development Permit prior to:

- a) alteration of land;
- b) subdivision; or
- c) construction of, addition to, or alteration of a building or structure. For information, development as defined in the Riparian Areas Regulation means any of the following associated with or resulting from the local government regulation or approval of residential, commercial, or industrial activities or ancillary activities:
 - d) removal, alteration, disruption or destruction of vegetation;
 - e) disturbance of soils;
 - f) construction or erection of buildings or structures;
 - g) creation of non-structural impervious or semi-pervious surfaces;
 - h) flood protection works;
 - i) construction of roads, trails, docks, wharves, and bridges;
 - j) provision and maintenance of sewer and water services;
 - k) development of drainage systems;
 - l) development of utility corridors;
- m) subdivision as defined in Section 455 of the Local Government Act

Exemptions

A Development Permit is not required if the proposed project or development consists only of:

- a) Actions and activities performed by federal, provincial or City staff or their contractors to prevent, control, or reduce flooding, erosion, slope failure or other immediate threats to life or property, including:
 - b) emergency flood or erosion protection works;
 - c) clearing of an obstruction from a bridge, culvert or drainage flow;
 - d) repairs to bridges or safety fences.
 - e) The emergency removal of dangerous or hazardous trees or tree limbs;
 - f) The implementation or construction of a fish habitat restoration plan previously authorized by the relevant federal or provincial agencies;
 - g) The construction or maintenance of public facilities by federal, provincial or City agencies; and
 - h) A renovation or alteration that does not expand the existing building footprint; and in the case of an exterior renovation or alteration does not include any action that would be considered a disturbance of the feature being protected
 - i) for farm or agricultural activities which are subject to the Farm Protection (Right to Farm) Act (where the Riparian Areas Regulation does not apply to agriculture). Other legislation such as the Federal Fisheries Act or the Provincial Water Sustainability Act may apply to farm or agriculture activities.

Waivers

Before making application for a Development Permit, property owners or their agents may submit a "Waiver Application" to determine whether the proposed development qualifies for a waiver of the Development Permit process. Qualifying projects may be issued a waiver in the following circumstances:

- a. The registration of a restrictive covenant on the subject property by the owner in favour of and to the satisfaction of the City that protects the riparian assessment area from development as defined in the Riparian Areas Regulation and specifies how the area is to be protected and/or maintained.
- b. Notification to the City by the Ministry of the Environment that an assessment certified by a Qualified Environmental Professional to identify the streamside protection and enhancement area has been approved in accordance with the Riparian Areas Protection Regulation and the registration of a restrictive covenant on the subject property by the owner in favour of and to the satisfaction of the City that protects the streamside protection and enhancement area identified in the assessment and specifies how the area is to be protected and/or maintained.

An applicant for a Waiver may be required to submit a plan prepared by a Qualified Environmental Professional (QEP) that identifies the high water mark (and/or top of ravine bank) and the boundaries of the riparian area assessment area and/or the streamside protection and enhancement area as identified in a certified assessment prepared by the QEP. The plan may be required to be a schedule as part of a restrictive covenant registered as a condition of a Waiver approval, and as such would need to be prepared by a BC Land Surveyor (BCLS).

Any breach of the Waiver terms may result in the cancellation of the waiver. Such cancellation may require that the property owner apply for a Development Permit. The granting of a Waiver does not absolve the applicant from other necessary approvals (e.g. Building Permits etc.). Where a Waiver cannot be granted, a Development Permit will be required before the project can proceed.

Guidelines

1. In issuing conditions relating to a Development Permit Waiver or in issuing Development Permit conditions, the City may specify how Development Permit objectives can be satisfied. This includes, but is not limited to, consideration of the following:

- a) Protect unique or special natural features such as land forms, rock outcroppings, mature trees and vegetation, drainage courses, wetlands, hilltops and ridge lines;
- b) Retain mature vegetation wherever possible;
- c) Use low-flow or drip irrigation systems that minimize the use of water;
- d) Where land and/or natural vegetation is disturbed or damaged, restore the area with plants indigenous to the area or other appropriate plants;
- e) Not obstructing or causing impediments to the channel or flow of a stream, creek, watercourse, ditch, drain, or sewer whether or not it is located on private property;
- f) Retain, in a largely undisturbed state throughout and after the development process (unless there exists a hazardous condition which can only be addressed by disturbing the site), an area of land immediately adjacent to the watercourse or stream as defined in the Riparian Areas Regulation. The area to remain free of development is referred to as the riparian assessment area.

2. The width of the riparian assessment area shall be as defined in the Riparian Areas Protection Regulation. Relaxation of a SPEA may be considered only by notification to the City by the Ministry of Environment that an assessment certified by a Qualified Environmental Professional to identify the streamside protection and enhancement area has been prepared in accordance with the Riparian Areas Regulation.

3. The City may consider Zoning Bylaw variances, where necessary, in order to prevent, or minimize encroachment into the riparian assessment area. The changes that may be considered include, but are not limited to:

- a) Reducing setbacks,
- b) Increasing the allowable site coverage of buildings,
- c) Increasing maximum building height,
- d) Reducing parking space requirements.

4. In order to determine the location of an Environmentally Sensitive Riparian Areas Development Permit Area on a parcel, a property owner/agent may be required to submit a plan prepared by a BC Land Surveyor (BCLS) that identifies the high water mark (and/or top of ravine bank) and the boundaries of the riparian area assessment area and/or the streamside protection and enhancement area as identified in a certified assessment prepared by a Qualified Environmental Professional.

5. The City may, where a Development Permit is required, require registration of a restrictive covenant to specify how the lands are to be protected and/or maintained.

6. The City may, in order to achieve satisfaction of the above-noted guidelines, issue a Development Permit Waiver or Development Permit that:

- a) imposes conditions respecting the sequence and timing of construction.
- b) varies a bylaw dealing with subdivision servicing requirements (by Council) or zoning requirements (other than issues relating to use or density).
- c) includes requirements and conditions or sets standards for:
 - areas of land that must remain free of development, except in accordance with any conditions contained in the permit;

- specified natural features or areas to be preserved, protected, restored or enhanced;
- creek beds to be returned to the Crown;
- Riparian Areas to be protected;
- works to be constructed to preserve, protect, restore or enhance natural watercourses or other specified natural features of the environment including the incorporation of xeriscape (drought resistant, low water requirement) planting;
- protection measures, including that vegetation or trees be planted or retained in order to:
 - conserve, protect, restore or enhance fish habitat or riparian areas,
 - control drainage,
 - control erosion or protect stream/watercourse banks.

7. The City may, where a Development Permit Waiver or Development Permit is required, request the applicant to provide, at the applicant's expense, development approval information:

- a. Development approval information may be required if the effects of the proposed development in relation to Development Permit objectives, OCP policies, and other City bylaws and regulations cannot be fully assessed based on information otherwise available. Development approval information will be used to assist the City in determining conditions or requirements to be imposed in the permit;
- b. Development approval information may include, in accordance with the Riparian Areas Regulation, an assessment approved by the Province to identify the riparian assessment area and/or streamside protection and enhancement area; and
- c. Development approval information may include a plan prepared by a BC Land Surveyor (BCLS) that identifies the high water mark (and/or top of ravine bank) and the boundaries of the riparian assessment area and/or the streamside protection and enhancement area as identified in the certified assessment prepared by the Qualified Environmental Professional.

8. The City may, where a Development Permit is required, require the registration of a restrictive covenant on the subject property by the owner in favour of and to the satisfaction of the City of Salmon Arm that protects the SPEA area identified in a certified assessment prepared by a Qualified Environmental Professional and specifies how the area is to be protected and/or maintained.

B. Multi Family Residential Development Permit Area

Designation

Pursuant to Section 488(1)(f) of the *Local Government Act*, all land within the Medium and High Density Residential designated areas, as shown on Map A-1 Land Use, is designated “Multi-family Development Permit Area” and shall require a Development Permit (DP) for all residential development.

Objectives

To promote quality building, site and landscape design with high architectural standards that support the objectives of this OCP.

To ensure new multiple family housing development projects respond to and address local site conditions.

To promote landscape design requiring enhanced parcel permeability and tree cover.

To encourage safe pedestrian and vehicle access within and around multi- family developments.

To encourage the development of safe, functional, and livable multi family developments and neighbourhoods.

Application Submission Requirements

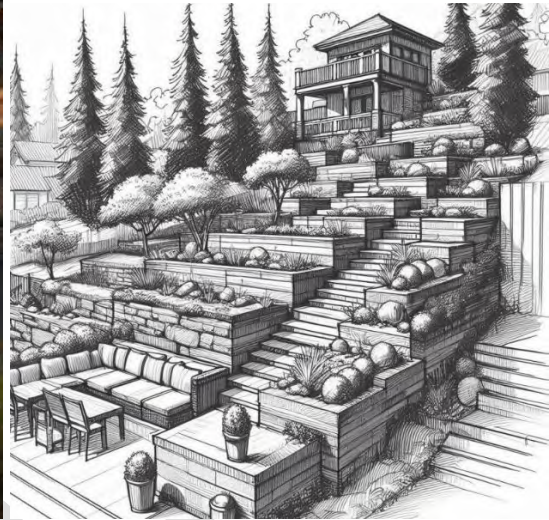
Development Permit application submissions must meet the following minimum requirements:

- a. Site plans must be scaled and include dimensioned parcel boundaries and setbacks; pedestrian and vehicular access and parking layouts; refuse and recycling container areas; and any mechanical equipment;
- b. Grading and drainage plans must indicate existing and proposed grades, proposed building floor elevations, coverage, layout, and elevations at top and bottom of steps and retaining walls; and surface materials with calculations of permeable surfaces, on-site infiltration areas and proposed drainage and stormwater management;
- c. Architectural drawings, prepared by a registered architect or qualified professional compliant with the BC Architect Act, must illustrate building design, massing, materials, finishes and colours;
- d. Landscape plans, prepared by a registered landscape architect or qualified/experienced horticulturalist, must indicate existing vegetation to be retained and protected; hard surfacing; steps and retaining walls; a planting plan and list showing the number, species and sizes of proposed plants. The extent and type of irrigation, and amenity features, e.g. site furniture, play areas, gathering areas; and address/directional sign plan;
- e. Applicants are recommended that prior to commencing a DP submission preparation, to consult with the City’s Planning Department to determine any site-specific requirements and considerations.

Greater Site and Building-Siting Guidelines

The design approach to the site and building siting shall:

- a. take advantage of existing site topography while retaining as much natural vegetation, especially mature healthy trees, and unique site features as possible;
- b. minimize the use of monolithic retaining walls where possible, and to break any retaining structures into stepped, smaller increments not exceeding 2.0 in height for any single retaining wall;



Stepped retaining walls

- c. situate the building(s) in relation to these site features and amenities, parks, greenways, etc. delineating clearly between public, semi-private and private spaces;



Public, private and semi-private spaces are clearly defined

- d. consider the safety and security of residents in the design and layout of the development with a focus on Crime Prevention through Environmental Design (CPTED) principles;
- e. strongly discourage walled or gated developments that completely block off visibility from the street;
- f. set parking away from the dominant street frontage with clear safe vehicle ingress/egress;



Screen parking areas from the street

- g. provide and design quality outdoors spaces for residents that foster social gatherings, physical fitness for all ages, structured or unstructured play, and/or gardening;



Create quality gathering spaces

- h. prioritize on-site outdoor children's play areas in landscaped areas for those developments located in excess of 400m of a public playground;
- i. create safe non-vehicular transportation connections from the dominant street frontage and access points to all of the buildings outdoor spaces;
- j. create new developments that are universally accessible and inclusive for all ages, abilities, and needs.



Create safe pedestrian connections to buildings

- k. create pedestrian and cycling connections that provide both internal connectivity and efficient links to existing or planned neighbouring sidewalks and trails on adjacent streets or adjacent developments;



Create safe bicycle connections

- l. Bicycle racks, lockers or indoor storage should be provided in secure areas with good visibility, access, and lighting, and located preferably near entrances. Exterior bike parking should be covered where possible; and
- m. minimize the impact of building shading on adjacent residences and outdoor use areas;

Architectural Guidelines

1. Design buildings with varied facades and visual interest or in a contemporary style of high quality finishes and careful proportions, with variation of facades with setbacks or projections of 0.75 metres or more; large scale, monolithic or bland blocks of buildings are discouraged.
2. The requirements for varied and articulated facades will be considered in conjunction with meeting the building performance requirements of the BC Energy BC Energy StepCode or Zero Carbon “Steps” and accommodations made to meet these requirements.



Articulate building massing

3. Designs can incorporate a wide variety of roof styles, provided rooftops are well organized and attractive when seen from above, neighbouring buildings and the street.

4. Design buildings with well defined entries and walkways from entries to the street or circulation systems, and consider weather protection over entry points, balconies and porches when possible.



Homes with defined entries that face the street

5. Design buildings with ground floor residential units that provide a front door and direct access to the exterior where possible.

6. All dwelling units should have easy access to useable private or semi-private outdoor amenity space.



Provide private and semi private spaces

7. Design buildings with parking garages or carports facing away from the street as much as possible, in order that the vehicular access is not a dominant design element.



Garages don't dominate the street

8. Use a mix of durable and high quality building materials with consideration to maintenance and the integration of natural materials where possible.

9. Design and finishes of accessory structures/buildings shall be consistent with the architecture of the principal buildings.

10. Enable natural light into interior spaces and outdoor use areas to reduce the energy needs, using passive solar principles where possible.

11. Consider the use of alternative technologies for on-site energy production, e.g. solar, micro wind turbines, geothermal, fuel cells and heat pumps.

12. Include areas for secure bike storage and parking in all multi family developments, particularly in apartments where each unit may not have direct access to the ground floor. Bicycle racks, lockers or indoor storage should be provided in a secured area with good visibility, access, and lighting, and located preferably near entrances. Exterior bike parking should be covered where possible.

13. Screen roof top mechanical equipment from views in a manner that is consistent with the architectural design of the building. Locations of ground level air conditioning units and HVAC mechanical equipment should be sited in a manner that does not impact adjacent residential lots. Screening of on-grade large mechanical equipment with noise and vibration abatement material is encouraged.

14. Exceptions to Development Permit Area guidelines or variances to building projections into minimum setback areas and minor variances to maximum building height established by the Zoning Bylaw may be considered for energy efficient buildings that commit to achieve BC Energy StepCode or Zero Carbon "Steps" in excess of the requirements of the Building Bylaw, in order to facilitate an energy efficient form of construction.

Landscape and Screening Guidelines

1. Maximize the amount of landscaped areas on site and minimize the amount of impervious paved surfaces to increase the natural infiltration (absorption) of rain water and to provide a more natural character.

2. Limit mowed ornamental grass lawn areas to highly visible areas and locations used for recreation in order to maximize areas for native, diverse and low maintenance vegetation.

3. Select trees and other plants that will be readily established and provide significant visual impact upon planting, without adversely affecting daylight or sunlight penetration into buildings or open spaces when fully grown.

4. Select and plant native and/or drought tolerant trees and plants suitable for the local climate, using the City's "Landscape Standards and Recommended Species Guide" as a reference.

5. Integrate trees into landscaping plans so that they provide adequate shade to any outdoor amenity, social areas and play spaces.

6. Design the landscape plan with user safety, maintenance and irrigation requirements in mind.

7. Developments are encouraged to incorporate native, low maintenance and xeriscape (drought resistant, low water requirement) concepts in landscape plans.

8. Encourage the use of water filtration/re-use systems that collect stormwater and rainwater for irrigation.

9. Design the landscape plan to limit the need for irrigation, and limit necessary irrigation to any landscape areas dedicated to food production.

9. Developments are encouraged to incorporate species and plantings in accordance with FireSmart BC Landscaping Best Practices.

10. Consider energy efficiency and conservation in landscape design, e.g. moderate wind, provide shade in summer, allow sunlight and daylight into buildings.

11. Locate walkways and amenity areas (plazas, courtyards, patios, etc.) away from vehicular traffic, with maximum accessibility to residential units, and accentuate them with landscaping.

12. Plant a uniform alignment of street trees along public streets at the spacing recommended by the City. Appropriate spacing is 15 metres along arterials, 10 metres along local and collector roads, lower spacing for smaller trees. If boulevard tree planting is not feasible along a street right-of-way, then tree planting along the front and exterior lot boundaries may be required at appropriate intervals. For street tree selection along public boulevards, use the City's "Landscape Standards and Recommended Species Guide" as a reference.



Provide street trees

13. Visual screening in the form of solid landscaping and/or fencing may be required along some segments of a site's perimeter, in particular along interior and rear lot lines and around outdoor storage areas.

14. Where landscaping for visual screening is required, plants selected shall be of sufficient height at maturity to provide a continuous screen not less than 2 metres in height and planted at a sufficient density to provide a hedge effect. Alternate screening measures such as decorative solid fencing or decorative walls not less than 2 metres in height may be considered instead of or in combination with planting.

15. Avoid using fences along street frontages to screen the development. Where a fence is unavoidable, use a transparent fence, e.g. lattice, metal, and add landscaping that allows views into and from the development.

16. Design location and directional signs (consistent with the City's address/directional sign guidelines), maps and mail box locations to be low profile, ground oriented and externally lit with low intensity fixtures accentuated by landscaping.

17. Locate refuse and recycling container areas and structures where they are accessible to residents and for servicing, screen them with an appropriate durable enclosure, and provide landscaping around the enclosure where possible. Avoid direct exposure of refuse and recycling areas to public streets.

18. Consider the provision of a composting system to generate materials that can be used for landscape maintenance or garden plots on the site.

Access, Circulation and Parking Guidelines

1. Design the internal road system to discourage speeding, and provide safe pedestrian routes from sidewalks and parking lots to building entrances.

2. Providing shared driveway access and shared parking areas should be consider whenever possible.

3. Parking areas shall be hard surfaced in accordance with the City's Zoning Bylaw.

4. Include areas for convenient guest parking when necessary. Smaller visitor parking areas, in close proximity to individual units or buildings, are favoured over large parking areas. Include areas design to accommodate snow collection and storage for the winter months (these areas can be shared with parking requirements).

5. Reduce the amount of asphalt paving and introducing other materials where possible, preferably permeable, e.g. permeable pavers, reinforced grass such as Grass-Pave.

6. Encourage bio-swales, permeable paving, and other design techniques that allow greater infiltration of water in and around parking areas.

7. Provide curb let-downs from accessible parking spaces to buildings.

8. Avoid parking areas where possible in front yards. In parking areas in excess of 20 stalls, intersperse intensively landscaped islands or bioswales at least 1.5 metres wide, planted with hardy vegetation and shade trees. Provide landscaping at the ends of parking rows, within and around parking lots as needed to define parking clusters, increase human comfort, provide visual relief, and increase infiltration of rain water.



Landscaped islands in parking areas

9. Encourage shared driveways to units with individual vehicular access.
10. Encourage underground or below grade parking that is well-lit and secured.
11. Include low intensity lighting in parking areas, designed and constructed for safety and convenient pedestrian and vehicular access.
12. Design on-site lighting to minimize glare and overspill onto adjacent properties, and into the sky. Submission of a detailed lighting plan may be required.

Exemptions

Development permits under this section are not required for the following:

- a. subdivision;
- b. interior renovations;
- c. exterior renovations which do not require a building permit;
- d. an addition of less than 100 square metres of floor area, provided the design is consistent - as determined by the City - with the form and character of existing buildings;
- e. an accessory building of less than 100 square metres of floor area, provided the design is consistent - as determined by the City - with the form and character of existing buildings;
- f. development of a duplex that is not part of a multiple unit development, strata or fee simple;
- g. development of a single family dwelling, a secondary suite and/or accessory dwelling unit qualifying that the Infill Residential Development Permit Area may apply.

C. Infill Residential Development Permit Area

Designation

Pursuant to Section 488(1)(f) of the *Local Government Act*, all land within the Low, Medium and High Density Residential designated areas shown on Map A-1 Land Use, is designated “Infill Residential Development Permit Area” and shall require a Development Permit for cases where three (3) or four (4) detached dwelling units are proposed to be developed on a single parcel where permitted by the Zoning Bylaw.

Objectives

To positively contribute to and integrate quality higher density ground-oriented housing within the context, scale and fabric of the streetscape in mainly large-lot residential neighbourhoods.

To secure well-considered landscaping and site planning that provides quality private outdoor living space for each dwelling unit; provides a clear transition between the private and public realm; and protects and preserves mature trees.

To ensure that building design including massing, cladding materials and window placement are well considered and contribute positively to the neighbourhood without unduly compromising the privacy of adjacent dwelling units and properties.

To secure clearly identified and accessible pedestrian and emergency services access to all dwelling units as well as the provision of adequate servicing, on-site parking and safe vehicle access from the public road.

Submission Requirements

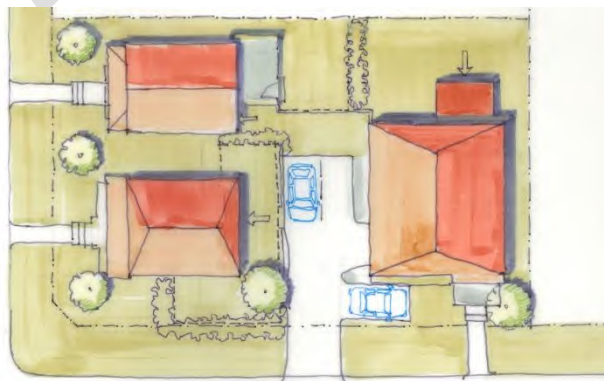
For required drawing submissions, see the requirements of the Multi Family Residential Development Permit Area (DPA) section. This Infill Residential Development Permit Area shall have the same requirements.

Streetscape and Site Concept Guidelines

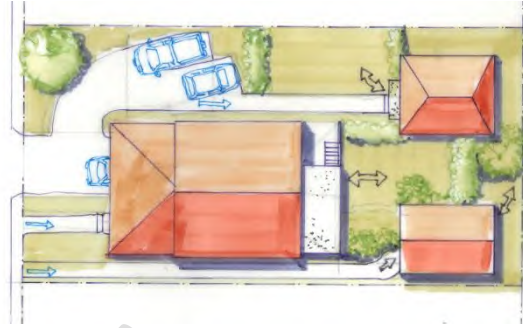
1. Respect and be generally consistent with the area streetscape by considering surrounding building heights, scale, and setbacks - despite build-out limits allowed by zoning. For example, limit significant height differences between proposed and existing development by stepping down dwelling massing to reflect the scale of surrounding homes.

2. Overall design should respond to site characteristics; for example, taking advantage of views, topography and solar/wind orientation. Consider the location of adjacent pedestrian/trail/ cycling networks.

3. Individual dwellings must not comprise of identical repeated designs; instead, they should be distinct in massing with some variation of unit size and façade design - all while achieving cohesion on the overall parcel.



4. Infill dwellings should improve upon and reflect the attributes of adjacent homes (e.g.: roof styles, porches, entrance features, materials). Where a neighbourhood may contain neglected properties, the development should set an improved standard for infill going forward.



5. Individual dwelling units should face and have entrances oriented to and visible from a fronting street wherever possible. In the case of parcels with flanking or double frontages, building design should also address these streets through the orientation of entries and windows.

Landscape and Site Plan and Guidelines

1. Design the site and locate buildings so as to protect mature trees, where possible; and, if trees cannot be protected or if there are no trees, ensure that adequate space will protect root systems and allow shade trees to reach mature size.

2. Consider landscaping strategically, for example: to frame building entrances; soften edges; screen parking and waste bins; break up long building elevations; enhance privacy; and manage snow and storm water storage.

3. Maximize permeable surfaces across the entire parcel. Developments with extensive nonporous concrete, asphalt, and solid roofs will not be supported. Use low fencing, gates, landscaping and a modest change in grade to define transitions from the public right of way to the private realm.

4. Link unit entrances to public sidewalks and parking via an accessible path of a minimum 1.2 metres width, and be adequately illuminated for pedestrian safety but not cast nuisance light into other dwellings and yards (see Figure 1).

5. Design and delineate a semi-private sheltered or recessed transition space at the front door of each dwelling unit. Carefully consider unit way-finding to minimize confusion for visitors (or emergency services).

6. Design each dwelling unit to have direct access to its own designated private outdoor amenity space that maximizes sun and daylight and can be used year-round.

Maximize the privacy of units on site and on neighbouring properties by minimizing “overlook” and direct sight lines between dwellings through strategies such as:

- off-set window locations in facing walls;
- consider dormers or clerestory windows for light;
- use of landscaping or screening;
- locate doors and patios to minimize potential noise and nuisance between units; and
- setback, angle or articulate facades where windows may compromise privacy.



Figure 1
Down-lights & signs at access path to units

7. Developments are encouraged to incorporate species and plantings in accordance with FireSmart BC Landscaping Best Practices.

Building Design Guidelines

1. Ensure that each building is unique, but the development remains cohesive, sharing an architectural style, proportions, and material palette.
2. Consider focal points at entrances, generous glazing, porches and other architectural features to emphasize the ground floor as the highest design priority.
3. Avoid blank monolithic walls by incorporating windows and articulating the façade with well-proportioned recesses/projections, reinforced by building material changes and with two or at most three durable, quality exterior cladding materials. Vinyl siding is discouraged.
4. The requirements for varied and articulated facades will be considered in conjunction with meeting the building performance requirements of the BC Energy BC Energy StepCode or Zero Carbon “Steps” and accommodations made to meet these requirements.

Servicing, Access and Parking Guidelines

1. Ensure that vehicle access is via one shared driveway off the secondary street or lane, where available, and minimize the impact of headlights shining into unit windows. Where vehicle access is only available via the primary street, provide for safe access/egress, eliminating the need for vehicles to back onto the public road.
2. Ensure that on-site parking is integrated in one common area or structure, preferably enclosed or screened, to eliminate the need to access parking and garages within individual units.
3. Minimize internal vehicle circulation. Where it is necessary, consider that it may serve as additional shared amenity space using strategies such as quality permeable materials (e.g. interlocking, permeable pavers), providing useable gathering and playing areas (and not large, monolithic paved areas).
4. Ensure the common utility and water service location is protected and remains accessible for service providers.
5. Ensure the site provides for combined solid waste pick-up/bin storage that is enclosed or otherwise screened from view.

D. Downtown Commercial Development Permit Area

Designation

Pursuant to Section 488(1)(f) of the Local Government Act, all land located within the Downtown designated area, as shown on Map A-1 Land Use, is designated "Downtown Development Permit Area".

Objectives

To promote quality building, site, landscape and streetscape design with the highest level of architectural standards.

To distinguish the Downtown as a leading precinct in urban design, supporting high levels of safe and varied pedestrian use and social interaction.

To provide the City with the ability to tailor new Downtown commercial development to local site conditions.

To promote environmentally sensitive building and landscape approaches.

Submission Requirements

Drawings submitted for a Development Permit application must meet the following minimum requirements:

Site plans must include:

- a. lot dimensions and setback dimensions taken from the building to all property lines; pedestrian and vehicular access and parking layout, with dimensions for parking stalls and travel aisles; solid waste and recycling container areas; any mechanical equipment at grade; and all surface materials such as paving and landscape areas;
- b. Grading and drainage plans indicating existing and proposed grades, proposed building floor elevations, location, layout, and elevations at top and bottom of all steps and retaining walls; and on-site infiltration areas and storm drainage;
- c. Drawings shall clearly indicate the materials, finish and colour of the buildings;
- d. A landscape plan must indicate existing vegetation to be retained and protected; hard surfaces; steps and retaining walls; mowed and rough grass areas; planting areas; and a planting plan with a plant list showing the number, species and sizes of proposed plants; extent and type of irrigation, and landscape amenity features, e.g. site furniture, gathering areas;
- e. Landscape design should integrate xeriscape and / or species that are suited to the local environment and the site conditions. Use of irrigation beyond landscape establishment is not encouraged;
- f. Building drawings shall be prepared by a registered architect;
- g. Landscape plans shall be prepared by a registered landscape architect or a landscape designer.

Siting and Building Guidelines

1. Design buildings to be consistent or complementary with the proportion, scale and massing of commercial buildings adjacent to the new development.
2. Design buildings that are comfortable and interesting for pedestrians rather than monolithic building massing.

3. Use quality and durable building materials to emphasize character and maintain visual continuity.
4. Encourage outdoor spaces that permit an activity in the building to extend onto the sidewalk without obstructing pedestrian flow, e.g. patios, courtyards, terraces, landscaped areas.
5. On corner sites, locate the principal building entrance at the corner, and design this as a reference point or landmark. Where building entrances cannot be located at the corner, provide a landscaped area with seating and special features at the corner.
6. Consider the safety and security of pedestrians in the design and layout of the development with a particular focus on the following Crime Prevention Through Environmental Design (CPTED) principles:
 - Natural Surveillance: a design strategy that is directed at keeping people under observation. It is based on a simple premise that a person will be less likely to act improperly if he or she can be seen. Natural surveillance is commonly associated with the establishment of clear sightlines. Natural surveillance can be complemented by mechanical forms of surveillance (closed circuit television) and/or organized forms such as security and police patrols.
 - Natural Control Access: is based on the simple premise that a person who is confronted with a clearly defined and/or strategically developed boundary, will typically show it some deference by respecting the way it guides and influences their movement as they transition from public through private space. Natural forms of access control includes fences, low walls, landscaping, gates and any barrier that is natural for the environment including topographical features, sales counters and even distance.
 - Territorial Reinforcement: is a design concept that realizes that physical design can create or extend a sphere of influence so that users develop a sense of proprietorship that is noticeable to the offender. It creates clear “public,” “semi-public,” and “private” spaces.
7. Site buildings to maximize available sunlight in public spaces.
8. Design buildings with segments to accommodate multiple storefronts. Avoid a continuous facade of more than 10 metres without a separately defined break or articulated portion.
9. The requirements for varied and articulated facades will be considered in conjunction with meeting the building performance requirements of the BC Energy StepCode or Zero Carbon “Steps” and accommodations made to meet these requirements.
10. Use local, durable and high quality building materials, integrating natural materials such as local stone, brick, and wood, as well as low-e glazing.
11. Screen roof top mechanical equipment from views in a manner that is consistent with the architectural design of the building. Do not locate intakes and outlets for ventilation adjacent to pedestrian walkways.
12. Create new developments that are universally accessible and inclusive for all ages, abilities, and needs.



Legend

1. Building façade divided into segments to accommodate multiple storefronts
2. The use of local materials and earth tone colours
3. Sign banner consistent with adjacent building
4. Decorative treatment of second floor windows blends with street level façade treatment
5. Creative sign applications for corner buildings where applicable
6. Pitched roof design blends well with adjacent structure
7. Transom windows on second unit provide interest
8. Rooftop HVAC equipment is concealed
9. Existing building with new façade treatments
10. New three storey building provides height and massing consistency along the street

Figure 1: Infill Building Design



13. The design of this building incorporates key desired principles such as:

- a pitched roof (6:12 – 12:12) with accents such as dormer windows
- a façade that is divided into smaller sections of 9 to 16 metres to provide an appropriate sense of scale and massing
- a setback for landscape and retail opportunities
- an architectural accent (for a building on a corner)



A good example of an interesting roof and a coordinated façade



A good example of the use of material – the main entrance is accentuated with stone, the façade is limited to two primary materials, stucco and stone

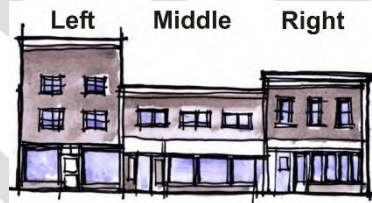


The use of local material such as stone on this building helps to create a strong identity and character in the City Centre

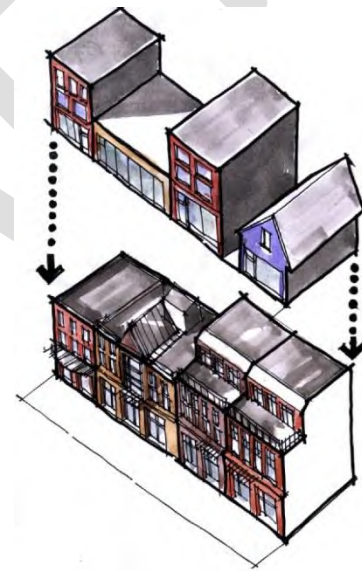
Figure 2: New Building Design



Photos A and B illustrate good examples of treatments for commercial building entrances, signs, and roof design



Left: a non-defined composition of the façade and poor proportions of the openings
 Centre: horizontally composed façade does not contribute to a small city streetscape
 Right: vertical composition of the openings provides great street rhythm and building proportions



Evolution from a streetscape with a mix of inconsistent massing and building proportions to a more harmonious and human-scale urban environment with well-designed architecture.

Figure 3: Building Treatment

Façade Design Guidelines

1. Provide unified architectural detailing on facades.
2. Use building materials and colours that complement the existing architecture and character of the Downtown.
3. Design the ground floor with direct visual and physical access between the ground level and the street, with pedestrian-oriented uses and easily identifiable pedestrian access points.
4. Avoid blank walls on the first two storeys facing pedestrian areas, encouraging windows, detailing and art. Continue windows on higher levels facing pedestrian areas.
5. Maintain a consistent grade between the sidewalk and entrances or public areas in front of buildings, with barrier-free access to primary building entrances.
6. Provide weather protection for pedestrians through the use of canopies, awnings or arcades.
7. Incorporate signs as an integral part of façade design, coordinating with adjacent buildings where possible.
8. Design building facades to step back from the street above 2 storeys, considering the need to meet the building performance requirements of the BC Energy BC Energy StepCode or Zero Carbon “Steps”.
9. Locate facades at the setback line, stepping back only for usable urban spaces or special features.



Figure 4: Façade design

Collectively, façade design can strengthen the pedestrian environment, enhance the visual character, and improve the economic and social vitality of a city block.



Façade design can include complementary materials that provide texture and contrast to add a sense of richness to the streetscape. Generous window placement also helps to create a successful retail environment.



A quality storefront design can greatly enhance the image of the retail business and the overall character of the street



The placement of windows and doors can be used to establish a rhythm along the street



Architectural detailing, signage and landscaping provide a well-defined and pleasing entrance



A good example of a façade redevelopment in the Downtown

Figure 5: Façade Design



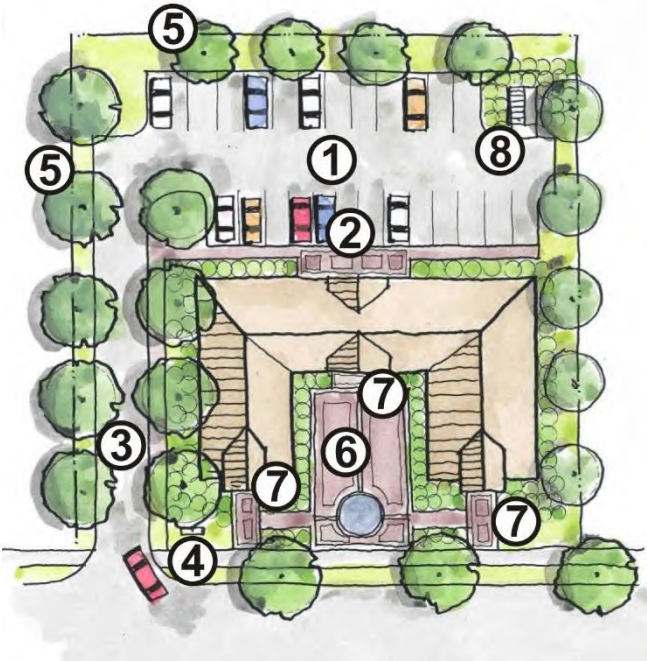
Legend

1. Base panel or kick plate with architectural material to match character of building and local context
2. Storefront door
3. Display window to promote visibility of retail goods and indoor activity
4. Sign band for prominent display of retail establishment
5. Sign lighting with external light fixtures
6. Transom window
7. Colonnade pilaster and colonnade to provide all weather comfort for pedestrians
8. Glass or opaque canopy
9. Wood trim, accents and heavy wood timbers of pilasters and columns add character

Figure 6: Storefront/Façade Design

Landscape Design Guidelines

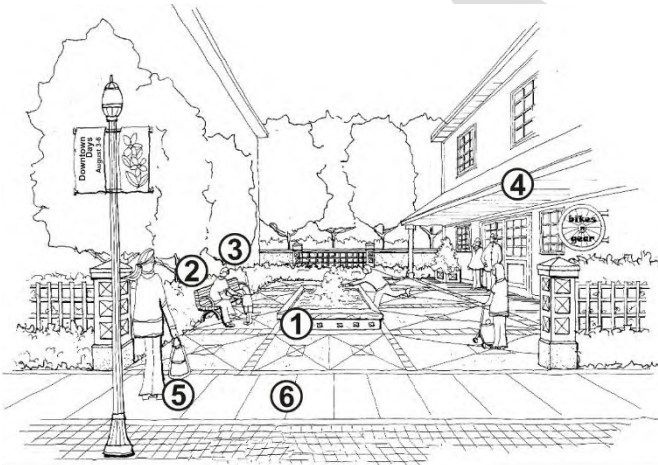
1. Design with an emphasis on street trees and landscaping.
2. Provide landscaped areas that have special and seasonal interest.
3. Integrate public and private pedestrian and landscaped spaces.
4. Encourage public art in pedestrian and landscaped spaces.
5. Use quality materials for hard landscape construction to complement the high quality materials of the buildings.
6. Incorporate pedestrian scale lighting in outdoor use areas.
7. Select trees and other plants that will be readily established and provide significant visual impact upon planting, without adversely affecting daylight or sunlight penetration into buildings or open spaces when fully grown.
8. Select and plant native and/or drought tolerant trees and plants suitable for the local climate, using the City's "Landscape Standards and Recommended Species Guide" as a reference.
9. Developments are encouraged to incorporate species and plantings in accordance with FireSmart BC Landscaping Best Practices.
10. In the landscape plan, consider finished site grades, utilities, views, shade and sun angles, needs for privacy or screening, user safety, maintenance and irrigation requirements, and all other typical site planning criteria.
11. Plant a uniform alignment of street trees along public streets at the spacing recommended by the City. Appropriate spacings are 15 metres along arterials, 10 metres along local and collector streets, lower spacing for smaller trees. If boulevard tree planting is not feasible along a street right-of-way, then tree planting along the front and exterior lot boundaries may be required at appropriate intervals.
12. For street tree selection along public boulevards, use the City's "Landscape Standards and Recommended Species Guide" as a reference.
13. Perimeter landscaping to reduce the visual impact of parking may be required in some situations. This will typically be a combination of grass, low shrubs and deciduous trees to retain visibility.
14. Locate refuse and recycling container areas where they are accessible to businesses and to container pick-up trucks, screen them with an appropriate durable enclosure, and provide landscaping around the perimeter of the enclosure where possible. Avoid direct exposure of refuse and recycling areas to public streets.



Legend

- 1. Parking to the rear of building
- 2. Pedestrian access at rear of building from parking
- 3. Two-way vehicular access drive
- 4. Opportunity for entrance and signage identification
- 5. Perimeter landscaping to screen parking and provide a buffer from adjacent properties
- 6. Opportunity for additional retail space or courtyard with pedestrian elements and landscape features
- 7. Identifiable building entrances
- 8. Utilities and storage contained and screened

Figure 7: Site Design



Legend

- 1. Focal point of courtyard – sculpture, water or landscape feature
- 2. Lighting, benches and site furniture
- 3. Vegetation to provide interest and comfort
- 4. Access to buildings and adjacent facilities such as parking
- 5. Courtyard is open to the street to create a safe environment and promote retail activity
- 6. Accessible surfaces to accommodate non-vehicular access from the street

Figure 8: Courtyard Design



Buildings with well-designed landscape treatments and rear lot parking contribute to a well designed, green and inspiring streetscape

Figure 7: Landscape Treatment



A colonnade can be constructed to improve non-vehicular access from rear lot parking areas

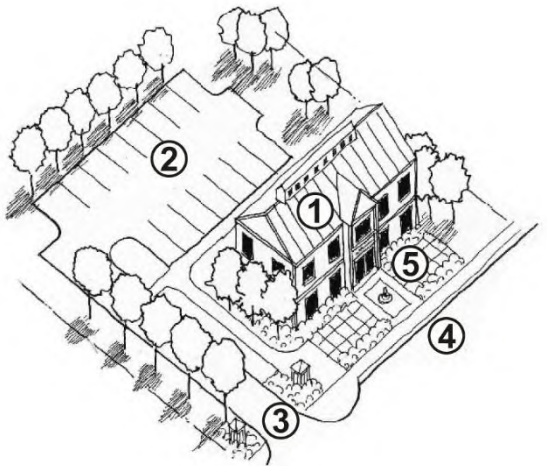
A building setback area can be used to maximize retail space and provide opportunities for landscaping

Figure 9: Building Setback Area

Access, Circulation and Parking Area Guidelines

1. Minimize potential pedestrian and vehicular conflicts through well defined streets and parking lot accesses that respect pedestrian corridors. Develop shared vehicular access points and parking lots where possible.
2. Locate parking lots at the rear of lots, or to the sides of buildings where required and no parking is available in the rear.
3. Design parking lots for efficient circulation of all types of vehicles, with a layout that discourages speeding and provides safe pedestrian routes from parking lots to sidewalks and building entrances.

4. Parking areas shall be hard surfaced in accordance with the Zoning Bylaw.
5. Maximize the amount of landscaped areas on site and minimize the amount of impervious paved surfaces to increase the natural infiltration (absorption) of rain water and to provide a more natural character.
6. Encourage bioswales, permeable paving, and other design techniques that allow greater infiltration of water in and around parking areas.
7. Provide curb let-downs to accommodate universal accessibility from disabled parking spaces to sidewalks and buildings.
8. In parking areas in excess of 20 stalls, intersperse intensively landscaped islands or bioswales at least 1.5 metres wide, planted with hardy vegetation and shade trees. Provide landscaping at the ends of parking rows, within and around parking lots as needed to define parking clusters, increase human comfort, provide visual relief, and increase infiltration of rain water.
9. Encourage underground or below grade parking that is well-lit and secured.
10. Include low intensity lighting in parking areas, designed and constructed for convenient pedestrian and vehicular access.
11. Design on-site lighting to minimize glare and overspill into the development, to adjacent properties, and into the sky. Submission of a detailed lighting plan may be required.
12. Rear lot parking requires a well-defined entrance and access for pedestrians and vehicles.



Legend

1. Building with strong visual connection to the street
2. Parking at rear of building
3. Two-way vehicular access
4. On-street parking where permitted
5. Landscape opportunity with direct connection to and access from the street

Figure 10: Parking Layout and Design



Figure 11: Streetscape

A combined driveway entrance can be incorporated to reduce potential pedestrian and vehicle conflicts. Organized traffic and vehicular access provides improved streetscape opportunities.

Exemptions

Development permits are not required in the Downtown Commercial Development Permit Area for:

interior renovations;

- a. an exterior renovation which does not require a building permit;
- b. an exterior addition with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing buildings as determined by the Planning Department;
- c. an accessory building with less than 50 square metres of floor area, in which the design is consistent with the form and character of the existing principal buildings as determined by the Planning Department;
- d. a subdivision application.

E. Highway Service/Tourist Commercial and Neighbourhood Commercial Development Permit Areas

Designation

Pursuant to Section 488(1)(f) of the Local Government Act, all land located within the Commercial - Highway Service/Tourist designated area, as shown on Map A-1 Land Use, and all land located outside the Commercial - Highway Service/Tourist designated area zoned C-5 (Tourist Commercial Zone), is designated "Highway Service/Tourist Commercial Development Permit Area, and all land located within the Commercial -Neighbourhood designated area, as shown on Map A-1 Land Use, is designated "Neighbourhood Commercial Development Permit Area".

Except where specifically noted in this section, the following provisions relate to both Highway Service/Tourist Commercial and Neighbourhood Commercial Development Permit Areas.

Objectives

To promote quality building, site and landscape design with high level architectural standards.

To ensure that commercial developments provide a positive impression of Salmon Arm along provincial highways, city roads and in residential neighbourhoods.

To encourage new commercial developments to promote safe non-vehicular access.

To promote environmentally sensitive building and landscape approaches.

Submission Requirements

Drawings submitted for a Development Permit application must meet the same minimum requirements as those specified in the Downtown Commercial Development Permit Area Section.

Siting and Building Guidelines

1. Design the site layout and building locations to:

- a. retain and protect important natural vegetation, rocks, and unique site features, including unique tree species, mature trees that are not a hazard, other significant vegetation;
- b. encourage the penetration of sunlight and natural light into interior spaces to reduce the energy needed for lighting and heating, using passive solar siting principles;
- c. work with the existing topography, minimizing the need for cut and fill or tall retaining walls.

2. Orient building frontages and main entrances to the dominant street frontage, with well defined entries with walkways and bicycle access to the street.

3. Locate parking lots at the rear of lots, or to the sides of buildings where required and no parking is available in the rear.

4. Design portions of buildings visible from a provincial highway or city street with architectural interest, with design features such as varying roof lines, extensive glazing, well defined entrances, business-specific signage, and high quality exterior elements.

5. Consider the safety and security of businesses and customers in the design and layout of the development with a particular focus on the following Crime Prevention through Environmental Design (CPTED) principles:

- **Natural Surveillance:** a design strategy that is directed at keeping people under observation. It is based on a simple premise that a person will be less likely to act improperly if he or she can be seen. Natural surveillance is commonly associated with the establishment of clear sightlines. Natural surveillance can be complemented by mechanical forms of surveillance (closed circuit television) and/or organized forms such as security and police patrols.
- **Natural Control Access:** is based on the simple premise that a person who is confronted with a clearly defined and/or strategically developed boundary, will typically show it some deference by respecting the way it guides and influences their movement as they transition from public through private space. Natural forms of access control includes fences, low walls, landscaping, gates and any barrier that is natural for the environment including topographical features, sales counters and even distance.
- **Territorial Reinforcement:** is a design concept that realizes that physical design can create or extend a sphere of influence so that users develop a sense of proprietorship that is noticeable to the offender. It creates clear “public,” “semi-public,” and “private” spaces.

6. Screen roof top mechanical equipment from views in a manner that is consistent with the architectural design of the building. Screening of on-grade large mechanical equipment with noise and vibration abatement material is encouraged.

7. Design facades and rooflines of accessory structures and buildings in a manner that is consistent with the architectural design of the principal buildings.

8. Create new developments that are universally accessible and inclusive for all ages, abilities, and needs.

9. Consider the use of alternative technologies for on-site energy production, e.g., geothermal, photo-voltaic and fuel cells, heat pumps.

Siting and Building Guidelines for Neighbourhood Commercial Development Permit Areas Only

1. Site and design buildings to be compatible with the form and character of the surrounding neighbourhood.

2. Minimize the visual, noise and traffic impacts of commercial activity on the surrounding neighbourhood.

3. Design buildings with varied facades and articulated roof lines, or design in a contemporary style that offers visual interest. The intent of this guideline is to discourage large bland buildings that do not reflect the character or scale of Salmon Arm.

4. The requirements for varied and articulated facades will be considered in conjunction with meeting the building performance requirements of the BC Energy BC Energy StepCode or Zero Carbon “Steps” and accommodations made to meet these requirements.

5. Use durable and high quality building materials, integrating natural materials such as local stone, brick, and wood, as well as low-e glazing.

6. Consider weather protection over entry points, balconies and porches, e.g., roof overhangs, or area inset below the floor above.

Landscape and Screening Guidelines

1. Maximize the amount of landscaped areas on site to increase the natural infiltration of rain water, with landscaping along adjacent streets and adjoining residential sites.

2. Design the landscape to define the edges of the site, minimize the view of parking lots from the streets and adjacent residential sites, and to provide year-round interest.

3. Consider hard surface paving other than asphalt adjacent to streets to create more interest and reduce the visual impact of the asphalt.

4. Select trees and other plants that will be readily established and provide significant visual impact upon planting.

5. In the landscape plan, consider finished site grades, location and heights of retaining walls, underground irrigation alignments, utilities, views, shade and sun angles, needs for privacy or screening, user safety, maintenance and irrigation requirements, and all other typical site planning criteria.

6. Consider energy efficiency and conservation in landscape design, e.g. moderate wind, provide shade in summer, allow sunlight and daylight into buildings.

7. Developments are encouraged to incorporate native, low maintenance and xeriscape (drought resistant, low water requirement) concepts in landscape plans.

8. Select and plant native and/or drought tolerant trees and plants suitable for the local climate, using the City's "Landscape Standards and Recommended Species Guide" as a reference.

9. Developments are encouraged to incorporate species and plantings in accordance with FireSmart BC Landscaping Best Practices.

10. Plant a uniform alignment of street trees planted along public streets at the spacing recommended by the City. Appropriate spacings are 15 metres along arterials, 10 metres along local and collector roads, lower spacing for smaller trees. If boulevard tree planting is not feasible along a street right-of-way, then tree planting along the front and exterior lot boundaries may be required at appropriate intervals.

11. For street tree selection along public boulevards, use the City's "Landscape Standards and Recommended Species Guide" as a reference.

12. Visual screening in the form of solid landscaping and/or fencing may be required along some segments of a site's perimeter, particularly adjacent to residential development.

13. Where landscaping for visual screening is required, plants selected shall be of sufficient height at maturity to provide a continuous screen not less than 2 metres in height and planted at a sufficient density to provide a hedge effect. Alternate screening measures such as decorative solid fencing or decorative walls not less than 2 metres in height may be considered instead of or in combination with planting.

14. Locate refuse and recycling container areas where they are accessible to businesses and to container pick-up trucks, screen them with an appropriate durable enclosure, and provide landscaping around the perimeter of the enclosure where possible. Avoid direct exposure of refuse and recycling areas to public streets.

Access, Circulation and Parking Area Guidelines

1. Design the internal road and parking system for efficient circulation of all types of vehicles, with a layout that discourages speeding, and provides safe pedestrian routes from parking lots to building entrances.
2. Parking areas shall be hard surfaced in accordance with the City's Zoning Bylaw.
3. Locate parking lots at the rear of lots, or to the sides of buildings where required and no parking is available in the rear.
4. Encourage reducing the amount of asphalt paving and introducing other materials where possible, preferably permeable, e.g., permeable pavers, reinforced grass such as Grass-Pave.
5. Encourage bioswales, permeable paving, and other design techniques that allow greater infiltration of water in and around parking areas.
6. Maximize the amount of landscaped areas on site and minimize the amount of impervious paved surfaces to increase the natural infiltration (absorption) of rain water and to provide a more natural character.
7. Provide curb let-downs to accommodate universal accessibility from disabled parking spaces to buildings.
8. Encourage shared parking lot accesses to adjacent developments.
9. Site buildings rather than parking lots on corners where possible.
10. In parking areas in excess of 20 stalls, intersperse intensively landscaped islands or bioswales at least 1.5 metres wide, planted with hardy vegetation and shade trees. Provide landscaping at the ends of parking rows, within and around parking lots as needed to increase human comfort, provide visual relief, and increase infiltration of rain water.
11. Design on-site lighting to minimize glare and overspill into adjacent residential properties, and into the sky. Submission of a detailed lighting plan may be required.
12. Access, Circulation and Parking Area Guidelines for Highway Service/Tourist Commercial Development Permit Areas Only
13. Do not locate parking areas in excess of 20 stalls between the street frontage and buildings. Locate large areas of parking at the rear or at the side of buildings.

Access, Circulation and Parking Area Guidelines for Neighbourhood Commercial Development Permit Areas Only

Do not locate parking areas in excess of 6 stalls between the street frontage and buildings. Locate larger areas of parking at the rear or at the side of buildings.

Exemptions

Development permits are not required in the Highway Service/Tourist Commercial and Neighbourhood Commercial Development Permit Areas for:

- a. interior renovations;
- b. an exterior renovation which does not require a building permit;
- c. a subdivision application.

Development permits are not required in the Highway Service/Tourist Commercial Development Permit Areas for:

- a. an exterior addition with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing buildings as determined by the Planning Department;
- b. an accessory building with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing principal buildings as determined by the Planning Department.

Development permits are not required in the Neighbourhood Commercial Development Permit Areas for:

- a. an exterior addition with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing buildings as determined by the Planning Department;
- b. an accessory building with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing principal buildings as determined by the Planning Department;
- c. a subdivision application.

F. Industrial Development Permit Area

Designation

Pursuant to Section 488(1)(f) of the *Local Government Act*, all lands designated for Light Industrial, General Industrial, or Airport Industrial/Commercial use as shown on Map A-1 Land Use, and fronting the Trans Canada Highway, Highway 97B, 10 Avenue SE, or the north side of 20 Avenue SE are designated “ Industrial Development Permit Area”.

Objectives

To promote quality building, site and landscape design.

To ensure that industrial developments provide a positive impression of Salmon Arm along provincial highways and city streets.

To promote environmentally sensitive building and landscape approaches.

Submission Requirements

Drawings submitted for a Development Permit application must meet the following minimum requirements:

- a. The site plan must include lot dimensions and setback dimensions taken from the building to all property lines; non-vehicular and vehicular access and parking layout, with dimensions for parking stalls and traffic aisles; refuse and recycling container areas, mechanical equipment at grade, and all surface materials such as paving and landscape areas;
- b. A grading and drainage plan must indicate existing and proposed grades, proposed building floor elevations, location, layout, and elevations at top and bottom of all steps and retaining walls; and onsite infiltration areas and storm drainage;
- c. Drawings illustrating the building appearance shall indicate the materials, finish and colour of the buildings;
- d. A landscape plan must indicate existing vegetation to be retained and protected; hard surfaces; steps and retaining walls; mowed and rough grass areas; planting areas; and a planting plan with a plant list showing the number, species and sizes of proposed plants; extent and type of irrigation, and landscape amenity features.

Development permit application drawings should be prepared by a registered architect or a draftsman with experience in urban design.

Landscape plans should be prepared by a registered landscape architect or a landscape designer.

Prior to formal site planning, applicants are encouraged to consult with the City's Planning Department to determine which natural features should remain on the development site.

Siting and Building Guidelines

1. Design the site layout and building locations to:
 - a. retain and protect important natural vegetation, rocks, and unique site features, including unique tree species, mature trees that are not a hazard, other significant vegetation, nesting areas, and other wildlife habitat;
 - b. work with the existing topography, minimizing the need for cut and fill or tall retaining walls; and
 - c. provide a buffer for surrounding residential developments.

2. Orient main entrances to the dominant street frontage, with well defined entries.
3. Design entrances and portions of buildings visible from a provincial highway or city street with some architectural interest.
4. Consider the safety and security of businesses and customers in the design and layout of the development with a particular focus on the following Crime Prevention Through Environmental Design (CPTED) principles:
 - **Natural Surveillance:** a design strategy that is directed at keeping people under observation. It is based on a simple premise that a person will be less likely to act improperly if he or she can be seen. Natural surveillance is commonly associated with the establishment of clear sightlines. Natural surveillance can be complemented by mechanical forms of surveillance (closed circuit television) and/or organized forms such as security and police patrols.
 - **Natural Control Access:** is based on the simple premise that a person who is confronted with a clearly defined and/or strategically developed boundary, will typically show it some deference by respecting the way it guides and influences their movement as they transition from public through private space. Natural forms of access control includes fences, low walls, landscaping, gates and any barrier that is natural for the environment including topographical features, sales counters and even distance.
 - **Territorial Reinforcement:** is a design concept that realizes that physical design can create or extend a sphere of influence so that users develop a sense of proprietorship that is noticeable to the offender. It creates clear “public,” “semi-public,” and “private” spaces.
5. Screen roof top mechanical equipment from views in a manner that is consistent with the architectural design of the building. Screening of on-grade large mechanical equipment with noise and vibration abatement material is encouraged.
6. Consider the use of alternative technologies for on-site energy production, e.g. geothermal, photovoltaic and fuel cells, heat pumps.

Landscape and Screening Guidelines

1. Maximize the amount of landscaped areas on site to increase the natural infiltration of rain water, with landscaping along adjacent streets and adjoining residential sites.
2. Select trees and other plants that will be readily established and provide significant visual impact upon planting.
3. In the landscape plan, consider finished site grades, location and heights of retaining walls, utilities, views, shade and sun angles, needs for privacy or screening, user safety, maintenance and irrigation requirements, and all other typical site planning criteria.
4. Consider energy efficiency and conservation in landscape design, e.g., moderate wind, provide shade in summer, allow sunlight and daylight into buildings.
5. Developments are encouraged to incorporate native, low maintenance and xeriscape (drought resistant, low water requirement) concepts in landscape plans.
6. Select and plant native and/or drought tolerant trees and plants suitable for the local climate, using the City’s “Landscape Standards and Recommended Species Guide” as a reference.

7. Developments are encouraged to incorporate species and plantings in accordance with FireSmart BC Landscaping Best Practices.
8. Plant a uniform alignment of street trees planted along public streets at the spacing recommended by the City. Appropriate spacings are 15 metres along arterials, 10 metres along local and collector roads, lower spacing for smaller trees. If boulevard tree planting is not feasible along a street right-of-way, then tree planting along the front and exterior lot boundaries may be required at appropriate intervals.
9. For street tree selection along public boulevards, use the City's "Landscape Standards and Recommended Species Guide" as a reference.
10. Visual screening in the form of solid landscaping and/or fencing may be required along some segments of a site's perimeter, particularly adjacent to residential development.
11. Where landscaping for visual screening is required, plants selected shall be of sufficient height at maturity to provide a continuous screen not less than 2 metres in height and planted at a sufficient density to provide a hedge effect. Alternate screening measures such as solid fencing not less than 2 metres in height may be considered instead of or in combination with planting
12. Locate refuse and recycling container areas where they are accessible to businesses and to container pick-up trucks, screen them with an appropriate durable enclosure, and provide landscaping around the perimeter of the enclosure where possible. Avoid direct exposure of refuse and recycling areas to public streets.

Access, Circulation and Parking Area Guidelines

1. Design the internal road and parking system for efficient circulation of all types of vehicles, with a layout that discourages speeding and provides safe pedestrian routes from parking lots to building entrances.
2. Encourage permeable materials for parking areas where possible, including gravel for areas that are used less frequently.
3. Encourage bioswales, permeable paving, and other design techniques that allow greater infiltration of water in and around parking areas.
4. Maximize the amount of landscaped areas on site and minimize the amount of impervious paved surfaces to increase the natural infiltration (absorption) of rain water and to provide a more natural character.
5. Provide curb let-downs to accommodate universal accessibility from disabled parking spaces to buildings.
6. Encourage shared parking lot accesses to adjacent developments.
7. Design on-site lighting to minimize glare and overspill into adjacent residential properties, and into the sky. Submission of a detailed lighting plan may be required.

Exemptions

Development permits are not required in the Industrial Development Permit Areas for:

- a. interior renovations;
- b. an exterior renovation which does not require a building permit;
- c. an exterior addition with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing buildings as determined by the Planning Department;
- d. an accessory building with less than 100 square metres of floor area, in which the design is consistent with the form and character of the existing principal buildings as determined by the Planning Department; or
- e. a subdivision application.

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G. Potential Hazard Areas Development Permit Area

Designation

Pursuant to Section 488(1)(b) of the *Local Government Act*, the areas shown as slopes over 30% and the 1 in 200 year floodplain on Map 15.1 Potential Hazard Areas, are designated as the "Potential Hazardous Areas Development Permit Area".

Objectives

To protect people and property from hazardous conditions such as flooding, mud flows, debris torrents, erosion, land slippage, rock falls and subsidence which may, in some cases, be reasonably abated with appropriate precautionary measures.

Application

Unless a Development Permit Waiver has been issued, all properties designated as Potential Hazardous Areas Development Permit Areas will require a Development Permit prior to:

- a) alteration of land; or
- b) subdivision; or
- c) construction of, addition to or alteration of a building or structure.

Waivers

Before making application for a Development Permit, property owners or their agents may complete a "Waiver Application" form to determine whether the proposed development qualifies for a waiver of the Development Permit process. Qualifying projects may be issued a waiver that may specify conditions. The City may also require registration of a restrictive covenant to specify how the lands are to be protected and maintained. Any breach of the waiver terms may result in the cancellation of the waiver. The granting of a waiver does not absolve the applicant from other necessary approvals, e.g. Building Permits. Where waivers cannot be granted, a Development Permit will be required before the project can proceed.

A Development Permit will not be required if the proposed development consists only of: Actions or activities performed by the federal or provincial government, or the City or their contractors to prevent, control, or reduce flooding, erosion, slope failure or other immediate threats to life or property, including:

- emergency flood or erosion protection works;
- clearing of an obstruction from a bridge, culvert or drainage flows; and
- emergency repairs to roads, bridges or any other municipal infrastructure.

A renovation or alteration that does not expand the existing footprint or increase the use or occupancy on the property; or

Actions which have been assessed by a qualified professional who has provided a report (to the satisfaction of the City) which concludes that the land is not subject to hazards.

Guidelines

1. In issuing conditions relating to a Development Permit Waiver or Development Permit, the City may specify how Development Permit objectives can be satisfied. This may include consideration of the following guidelines:

2. Require the registration of a restrictive covenant to save the City of Salmon Arm harmless from any future damage that may result from the hazard.

3. Require that development will be directed to areas with slopes of less than 30%. Where it can be demonstrated by an appropriate registered geotechnical professional that the proposed development will present no hazards to persons or property, development may be permitted on land with a slope greater than 30%.

4. Require that natural vegetation should be retained on slopes in order to control potential erosion, land slip and rock falls.

5. Require that an applicant wishing to propose development on slopes in excess of 30% or in areas of known geological hazard, to submit a report prepared by a registered geotechnical professional providing information on the anticipated on-site and off-site geotechnical impacts on the proposed development.

6. Consider and review geotechnical reports / plans for areas of known geotechnical hazard.

7. Require that an applicant wishing to propose development within the floodplain area, submit a report prepared by a registered hydrogeological professional providing information on the anticipated on-site and off-site impacts on the proposed development, including any transference of risk resulting.

8. Require that no buildings or structures be located within the Shuswap Lake, Canoe Creek or Salmon River floodplains as established by the City's Zoning Bylaw.

9. Require that no structural development be located below any flood elevation level established in the City's Zoning Bylaw.

10. The City may, in order to achieve satisfaction of the above noted guidelines, issue a Development Permit that:

- a) imposes conditions respecting the sequence and timing of construction;
- b) supplements a bylaw dealing with subdivision servicing requirements or zoning requirements;
- c) includes requirements and conditions or sets standards for:
 - i. areas of land that may be required to remain free of development, except in accordance with any conditions contained in the permit;
 - ii. an area that the permit designates as containing unstable soil or high ground water where the City requires that no septic tank, drainage and deposit fields or irrigation or water systems be constructed.

11. The City may, where a Development Permit is required, request the applicant to provide, at the applicant's expense, development approval information, in accordance with Section 484 of the Local Government Act. Development approval information may be required if the effects of the proposed development in relation to Development Permit objectives, OCP policies, and other City bylaws and regulations cannot be fully assessed based on information otherwise available, particularly with respect to potential impacts that may result on:

- a) municipal infrastructure;
- b) transportation patterns including traffic flow;
- c) public facilities including parks; and
- d) the natural environment.

Development approval information may be used to assist the City in determining conditions or requirements to be imposed in the permit.

H. Farm Protection Development Permit Area

Designation

Pursuant to Section 488(1)(c) of the Local Government Act, the Farm Protection Development Permit applies to all development lands adjacent to ALR Boundary (may be bisected by ALR Boundary on same parcel or road/trail/ etc.) or lands Agriculturally zoned.

Objective

To preserve and protect current and future food production within the City boundaries.

To limit the impact of adjacent residential, commercial, industrial or institutional uses that share boundaries with agricultural land

Application

All Subdivision or Building Permit applications on lands adjacent to the ALR Boundary (may be bisected by ALR Boundary on same parcel or road/trail/ etc.) or on lands Agriculturally zoned.

Guidelines

1. Urban-side development includes buildings, amenity and landscape areas, trails, roadways and utility corridors.
- 2 To implement land design and management to ensure minimal negative impacts or intrusions to agricultural-side by ensuring that the urban-side development includes:
3. In subdivision design density, road, and lot patterns should gradually transition to agricultural boundary, utilizing open space, landscaping and fencing to provide buffering between the urban-side development and agricultural land.
4. Dead-ending of roads and utilities to agricultural land is strongly discouraged.
5. Urban-side traffic patterns should avoid agricultural land boundaries. Buffers between roadways are strongly encouraged.
6. Amenity areas on the urban-side, without adequate physical buffers consistent with edge planning practices, are strongly discouraged.
7. Residential building locations should be at least 15m from the property line, 8m for commercial or industrial and 15m for institutional. Variances to setbacks may be supported when clustering of development prioritizes the gradual transition of urban-side development to agricultural lands.
8. Landscape plans for development on the urban-side avoid trees and shrubs that shade farm crops or are known to harbor insects or diseases harmful to crop production.
9. Select and plant native and/or drought tolerant trees and plants suitable for the local climate, using the City's "Landscape Standards and Recommended Species Guide" as a reference.
10. Developments and landscape plans are encouraged to incorporate species and plantings in accordance with FireSmart BC Landscaping Best Practices.

11. Storm and ground water management that are designed as coordinated between urban-side development and nearby farms are encouraged.

12. Best practices for buffer installation and maintenance provided within the 2015 Edge Planning Guide by the Ministry of Agriculture is encouraged for buffer and landscape areas on the urban-side development. All landscaping installed is to be maintained by the urban-side development.

13. Tools such as covenants or easements may be used to support the installation and maintenance of the buffer and landscape areas.

Exemptions:

Development permits are not required in the Farm Protection Permit Area for the following:

- a) Development on City owned land for the purpose of public use(s);
- b) Areas of slope greater than 30% or where topography or natural boundary provides a boundary between agricultural lot line and urban-side development of at least 15m;
- c) Areas separated by a street identified as an arterial or collector road;
- d) Construction, addition or alteration not exceeding 100m² where no variances to the Zoning Bylaw are required;
- e) Interior / exterior building alterations that do not expand the existing building foundation;
- f) Repair, maintenance, alteration or reconstruction of existing legal buildings, structures or utilities, providing there is no expansion of the footprint; or
- g) Replacement of a building that has been destroyed by natural causes, in cases where the replacement building is identical to the original in location, floor area and height

Temporary Use Permits

Designation

All lands designated as shown on Map A-1 Land Use, are designated Temporary Use Permit Area.

Objectives

As outlined in the Local Government Act, the Temporary Use Permit Area designation is intended to apply to uses which are temporary in nature and where the existing zoning does not permit the use.

Temporary Use Permit Policies

1. A Temporary Use Permit is a short-term zoning that permits uses on a specific property for a period of time set by Council for no more than a three year period. The proposed temporary use should be defined in the Zoning Bylaw and permitted in other zones, but a Temporary Use Permit may be considered for a use not defined, or not clearly defined in the Zoning Bylaw. Despite the zoning of a property, Temporary Commercial or Industrial Use Permits for temporary uses may be supported in the Temporary Use Permit Area, subject to approval by Council, or where delegated pursuant to the Development Procedures Bylaw.
2. Mineral and aggregate processing Temporary Use Permits may be permitted in areas of high aggregate potential, as identified on Map 6.2 Aggregate Potential or based on site investigations in other areas where the impacts can be effectively mitigated.
3. Guidelines for Temporary Use Permits include the following:
 - a maximum time of three years is required for the use;
 - appropriate parking and/or loading spaces are available;
 - the proposed hours, size and scale of the use will be compatible with adjacent land uses;
 - the use will be compatible with adjacent land uses in terms of noise, odours, dust, pollution, lighting, aesthetics, parking and traffic;
 - compliance with Provincial Environmental Management Act and Riparian Areas Protection Regulation;
 - the proposed use will not have negative impacts on the natural environment.
4. The City may require security in the form of a letter of credit and may impose reclamation and performance measures as conditions for the issuance of a Temporary Use Permit.
5. Specific permit conditions may address mitigation measures for potential negative impacts identified in the review process.
6. Non-residential Temporary Use Permits will not be considered in residentially zoned areas, except to allow for temporary uses and buildings connected and supporting a Building Permit issued by the City.

Submission Requirements

Submission requirements for Temporary Commercial Use Permits include but are not limited to:

- the proposed length of time of the use;
- access and the availability of parking and loading;
- the proposed hours of the use;

- the proposed size and scale of the use;
- any requested site servicing plans, site plans and building elevations and floor plans;
- description of noise, odours, dust, pollution, lighting, aesthetics, parking and traffic;
- appropriate safety and site control plan(s) approved by the RCMP and/or City Fire Department;
- Approval for proposed uses or development as defined by the Agricultural Land Commission or Ministry of Environment;
- Remediation or reclamation plan with appropriate estimates and;
- Mitigation of the potential impact of the proposed use on the natural environment.

Expiration of Permit

Upon expiration of a Temporary Use Permit, the uses of the property shall revert to those outlined in the current Zoning Bylaw. The applicant may, prior to the expiration of the Temporary Use Permit, apply for a permit renewal of up to three years, approval of which will be at the discretion of Council or staff where delegated pursuant to the Development Procedures Bylaw.

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